

1483

TRAMWAY GUIDE

TO

MELBOURNE AND
SUBURBS.

CONTAINING

ROUTES, FARES, REGULATIONS, &c.,

AND

RAMBLES FROM
THE ROUTES.

The Melbourne Tramway & Omnibus Co., Ltd.

OFFICES: 673 BOURKE STREET WEST.

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The Melbourne Tramway System

MELBOURNE, as commonly named, is like London, composed of many distinct localities under separate municipal governments, but as a whole making a large metropolitan centre.

The surrounding districts form a series of suburbs to the City of Melbourne proper, and of these the inner circle nine in number and eight of the outer ones are connected with the centre by the Tramway System and its feeding Omnibus Lines.

The Melbourne Omnibus Company began its services to the public by the initiation of Omnibus Lines in 1869, but having always a Tramway System in view, it changed its name in 1878, and from that time vigorously sought to introduce the more modern and popular Tramways. It took some years to educate the Public and the Governing Bodies to their necessity, but the Company's Tramway's Act was eventually passed by Parliament in 1883. Immediately thereafter the Managing Director, with an experienced engineer, visited Europe and America to enquire into the latest and most improved tramway systems. The result was the adoption of the Cable System as giving the greatest facilities for carrying a large traffic satisfactorily to the public.

The first line—that to Richmond—was opened in November, 1885, and the work being rapidly pushed on the others were opened at short intervals, and the present highly-approved system completed in 1890, with the exception of one short line which was delayed till 1891.

The complete Tramway System consists of 43½ miles of double-track cable lines, which use constantly over ninety miles of wire rope; and 4½ miles double-track horse lines.

. It is needless to dwell on the numerous advantages of the Tramway System of Melbourne, as they speak for themselves. Suffice to say that both Melbourne citizens and visitors all agree that the Company's Tramways are pre-eminent for cleanliness, comfort, agreeable motion, frequency and regularity of running, and a civil, attentive and capable body of employees.

The cars run at a higher speed than could be maintained by horses, and find no difficulty in surmounting grades and passing around curves. It is a satisfaction to passengers to know that for their convenience and comfort no tired horse is groaning under a heavy burden, and that the heaviest loaded car is drawn by the steam engines and wire ropes with perfect ease and no physical pain.

The tramway connection of all the suburbs with the city gives equal facilities for a visit to the beautiful hill and dale of the landward surroundings of favored Melbourne, or to its extensive and popular sea beaches at St. Kilda and South Melbourne. The crowded traffic to the seaside on summer evenings gives ample evidence that the general public is glad to avail itself of the opportunities thus afforded.

There has been a noticeable improvement in the appearance of the streets of Melbourne during the last ten years, through the better paving and the absence of mud and dust which has followed the advent of Tramways.

Moreover, property all along the lines has, during such ten years, largely increased in value. The low rates of fare charged enable all classes of city workers to have their homes in the pleasant suburbs, amid green fields and fresh air unvitiated with the smoke and grime of city life, thus aiding to a large extent in maintaining the cheerfulness and energy which is so marked a feature of the Melbourne resident.

In conclusion, that the Tramways of Melbourne fulfill their part in its many-sided life with general satisfaction is undoubted, and as the city moves on again with the incoming tide of prosperity so they will become, as in other cities, a still greater necessity than in the past.

The Cable Tramway.

THE Cable Tramway ranks, next to Edison's division and diffusion of the electric light, as one of the most useful discoveries of late times. Like that it is of American origin, its invention being due to difficulties in tramway traffic over the hilly quarters of San Francisco. The system has since been adopted in many of the American, and several English and European cities, but in none on so large a scale by one company as in Melbourne. To many of the daily arrivals, up country and intercolonial, in Melbourne, it will for years be a novelty, and need the explanations we hope to give.

THE CONSTRUCTION.

Prior to the construction of the cable tramway in Melbourne, it was necessary to create a Tramway Trust. This body is composed of seven members from the Melbourne City Council, and one member each from the councils of eleven of the surrounding municipalities interested in the Lines. The Trust proceeded to raise, by loans in London, sufficient funds to construct the sixteen cable tram tracks, and the engine houses by which they were to be worked, and the two horse tram lines. As they were completed the lines were leased to the Melbourne Tramway and Omnibus Company, Limited, whose capital was sufficient to build the carriages and equip the lines and engine houses with all necessary working requisites. The present lease to this Company expires in 1916. The Company pay to the Trust annually the interest due upon the loans raised, and also a sufficient annual sum as a sinking or redemption fund, to repay by its accumulation the principal of the loans raised by the Trust. At the expiration of the Company's Leases the loans will be fully repaid and the Lines will fall to the municipalities, represented on the Tramway Trust as a splendid property, which will have cost them nothing, and should bring in a substantial revenue through future leases.

THE TRACKS.

To the casual observer, when no tram cars are running, the tracks look as ordinary railway ones. Closer observation leads to a narrow slit, or slot, being seen away down the centre of each track. The ear perceives a rumbling noise coming from this aperture, and the eye being brought down close to it, sees a black-looking rope (the cable) running along, supported on small wheels about 30 feet apart: immediately over each wheel and on the track is seen an iron trap door; this, on being raised, shews an opening sufficient only for a small-made man to descend to the cemented tunnel, in which the wheels are fixed. By means of these traps all troubles with the cable and its supporting wheels receive immediate attention, and the tunnels are cleared from any droppings from above.

No ordinary observer, unacquainted with cable tramways, could possibly guess the meaning of what he saw, both above ground and below. Had the rope been running above ground he might have guessed that it would drag something along, but in this small tunnel the object of a rope running at 7 or 8 miles an hour and dragging nothing after it, would seem odd indeed.

THE CARS.

The cars coming along would only serve to remove the enquirer from off the track, but in no way clear up the mystery. Their progress would only add another wonder and further puzzlement. Nothing in the way of traction or propulsion given to the cars can be seen! How then were they moved along the rails, and moved so noiselessly, speedily and smoothly? Those who saw the first paddle wheel steamer on the Hudson were lost in wonderment to see a vessel without sails going along rapidly, and that in the teeth of the wind, but they saw the paddle wheels at work plainly enough, and it only remained to enquire what worked them, and what was the power vomiting forth sparks and smoke that was hidden below deck. With the tramcars no wheels are to be seen, and no sparks or smoke visible. In the Chinaman's words it is a case of "no pullee and no pushee." No evidence of steam or motor is

visible, and the power of stopping progress at will seems as unaccountable as the motive power. A guard in front acts as a dogcatcher, and a bright head light illuminates the tramway track by night.

THE GRIP.

Getting on the front or grip car, the bewildered enquirer would see between the side seats an upstanding man, whose movements would only recall to the sea voyager the "man at the wheel." This individual is seen to have the control of three levers, with the movements of which, evidently enough, the progress or stoppage of the cars entirely rests. But look keenly as he might, he could not see how by such means motion could be given to or taken from the cars. He would see further only that the levers were connected with a semicircle of iron, part of which sank through an opening in the floor of the dummy. If of enquiring mind, he might learn from a fellow passenger that this iron semicircle represented the top of "the grip," which curious name he would be told was given to the appliance, by which, through the bottom of the car, and through the slot between the rails, the running rope was clutched, or released at will, carried the car along, and that when released the car could be stopped with breaks. One part of the mystery was thus explained.

THE CABLE.

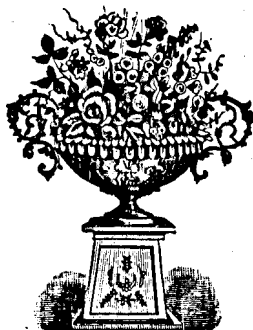
Arrived at the Engine House the mystery of the running cable would also be cleared up. It would there be seen to be an endless wire rope running on wheels, propelled by steam power. The cable would be found to run thence on its little wheel-supporters in the tunnel to the end of the track where it would enter the terminal pit, and winding around other wheels there would run into the tunnel of the second or return journey track, dragging with it the cars going in that direction. The mystery of cable tramways and their mode of traction would now be solved. It is but another triumph of steam power working. The cable itself is indeed a wonder in the way of showing what strength and elasticity can be put into wire-made rope but little thicker than a man's middle finger.

THE ENGINE HOUSE.

This building shows, primarily, gigantic wheels, around which, and in the groves of their broad surfaces, snakelike ropes are winding. The steam power which works these enormous wheels is generated in one part of the engine house, in a series of boilers built above large furnaces. The doors to these latter are occasionally open for coaling, and give then a shock to the beholder in the amount of heat thrown out and of coals shovelled in. The visitor to the Engine House will see there much that will in many ways stagger belief. In his "Joyce's Scientific Dialogues," and other school books, he would have learnt how heavy and how hard to pull a rope becomes, according to its length, when laid along the ground; also how impossible it is to pull along a pound weight tied to the end of a string half a mile in length. In these wheels and these thin-looking flexible ropes he sees all such theoretical matter put to the proof—and seemingly disproved! The ropes here seen run along for many miles, dragging not pounds, but tons upon tons of weight with them. That these slender ropes can do so much is a testimony to the strength of steel and the way it can be wound around an hempen core, and twisted about it as easily as silk. No extra weight, no additional cars or additions to the number they carry, at all affects the cable—it runs its steady course as smoothly as if it carried along only so many bluebottle flies. Having got the required rope it remained but to get the power necessary to pull it along for such prodigious distance, and to pull it along spite of all efforts of many cars encumbering it, weighting it and gripping it. This titanic power lies in steam applied to wheels, and it is about the most striking example of what steam power can do that can be anywhere seen! The endless rope that runs round these monster wheels in the engine house runs thence over smaller wheels on its way into the tunnels, in which it will run for miles to other large wheels far distant, around which it will turn on its way back, dragging its load of cars and heavy loads of passengers, seemingly without effort or strain.

THE BELL PUNCH.

As with everything else of the Cable Tramway System, this is also a novelty, and like many useful inventions, is of American origin. This most ingenious production, the Bell Punch, hangs like a London busman's badge, in front of the conductor. It is the recorder and registrar of all transactions between the conductor and the passengers. It is a check taker and account keeper during the whole time of the trip. The passengers are requested to note that it should sound "one bell" on receipt of each fare in coin, and on the punching of each ticket handed to the conductor.



C. LISTER & CO.,

THE OLDEST

Tea Blenders,

AND PACKERS OF INDIAN, CEYLON, AND
CHINA TEAS FOR HOME AND
EXPORT TRADE.

WHOLESALE AGENTS FOR

*The ASSAM and DARJEELING Pure
Indian Tea Company's Tea.*

*The Famed QUEEN'S GIFT Tea
Company's Packet Tea.*

OUVAH (*brand*) Ceylon Teas.
*Queen of all Teas; the Finest
Grown.*

DIRECT IMPORTERS OF TEAS AND COFFEES.

Stocks in Bond and Duty Paid of Indian, Ceylon, and
China Teas, including Souchong, Pekoe Souchong,
Pekoes, Panyongs, Paklum, True Oolongs, Kooloos,
and all Fancy Teas.

BONDED CUSTOMS NETT WEIGHTS CHARGED.

VERY LOWEST QUOTATIONS.

ESTABLISHED 1855.

ROUTE No. 1.—RICHMOND CABLE LINE.

TERMINI:—SPENCER ST. & HAWTHORN BRIDGE.

Length of Route, 3½ miles, Time between termini, 25 min.

BLUE BAND by Day. BLUE LIGHT at Night.

First Car leaves Hawthorn Bridge—Week Days at 5-47 a.m.;
Sundays at 1-30 p.m.

" " Spencer Street—Week Days at 6-14 a.m.;
Sundays at 1-58 p.m.

Last Car leaves Hawthorn Bridge—Week Days at 11-0 p.m.;
Saturdays, 11-30 p.m.; Sundays, 10-5 p.m.

" " Spencer Street—Week Days at 11-30 p.m.;
Saturdays, 12-0 p.m.; Sundays, 10-32 p.m.

PLACES OF INTEREST ON THE ROUTE:

Railway Station, Spencer Street.

Railway Viaduct, Flinders Street.

River Wharves, Shipping, and Custom Sheds.

City Fish and Produce Market.

City Refrigerating Stores.

Queen's Bridge and Princes Bridge.

Hobson's Bay and Princes Bridge Railway Stations.

Church of England Cathedral.

Metropolitan Gas Company's Offices.

Treasury and Fitzroy Gardens.

Jolimont and East Melbourne.

Melbourne and East Melbourne Cricket Grounds.

Richmond Park.

Richmond Town Hall.

Horticultural Society's Gardens.

Hawthorn Bridge and River Yarra.

*Connection made at Hawthorn Bridge Terminus with
Hawthorn Horse Line. Through Fare, 4d.*

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and St. Kilda,
Toorak, or Prahran.

At Market Street—Between Spencer or Swanston Streets, and
South or Port Melbourne.

SEE RAMBLES FROM THIS ROUTE.



THE
NOTED HOUSE
FOR

Boy's *
Clothing

IS

George

&

George's

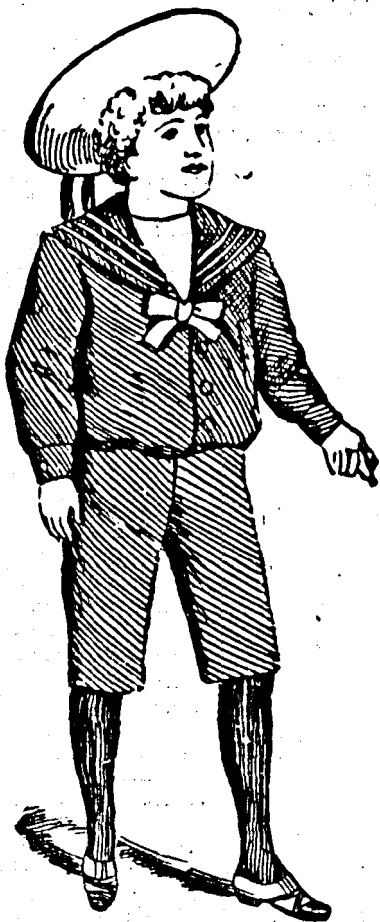
FEDERAL * *

EMPORIUM,

Collins Street

and

Lt1. Collins St.



Serge Sailor Suits from 4/11 Complete.

ROUTE No. 2.—VICTORIA STREET CABLE LINE.

TERMINI:—SPENCER STREET & VICTORIA BRIDGE.

Length of Route, 3 $\frac{3}{4}$ miles. Time between termini, 27 min.

RED BAND by Day. RED LIGHT at Night.

First Car leaves Victoria Bridge—Week Days at 5-40 a.m.;
Sundays at 1-30 p.m.

" " Spencer Street—Week Days at 6-7 a.m.;
Sundays at 1-58 p.m.

Last Car leaves Victoria Bridge—Week Days at 11-5 p.m.;
Saturdays at 11-35 p.m.; Sundays at 10-5 p.m.

" " Spencer Street—Week Days at 11-34 p.m.;
Saturdays at 12-4 a.m.; Sundays at 10-34 p.m.

PLACES OF INTEREST ON THE ROUTE :

Railway Station, Spencer Street,

Collins Street Banks and Office Buildings.

" The Block," Arcade.

Melbourne Town Hall.

The Athenæum Hall and Library.

The *Age*, The *Argus*, The *Herald* Newspaper Offices.

The " Scots," Baptist and

Congregational Churches.

The Melbourne Club. The Masonic Hall.

The Doctors' Quarter.

The Treasury. Government offices.

The Gordon Monument. Government Printing Office.

St. Patrick's R.C. Cathedral.

Victorian Art Gallery. The Cyclorama.

Central Fire Brigade Station.

Eye and Ear Hospital.

East Melbourne, East Collingwood, North Richmond,

Studley Park Foot Bridge.

Victoria Bridge. River Yarra.

*Connection made at Victoria Bridge Terminus with
Kew Horse Line.—Through Fare, 4d.*

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and Prahran,
St. Kilda or Toorak.

SEE RAMBLES FROM THIS ROUTE.

GEORGE & GEORGE

LIMITED,

ARE RENOWNED, FOR THE

STYLE, CUT, AND QUALITY

OF THEIR

Three Guinea Tweed Suits.

THEY ARE WITHOUT DOUBT

THE BEST VALUE IN MELBOURNE.

An Immense Assortment of

English, Irish, and Scotch Tweeds

TO CHOOSE FROM FOR

Their 16/6 Trousers.

Colonial Tweeds at lower prices.

George & George Limited,

FEDERAL EMPORIUM,

COLLINS STREET.

All Trams pass the Door or within two minutes walk.

Ask Conductor the nearest place to alight.

ROUTE No. 3.—FITZROY CABLE LINE.

TERMINI:—SPENCER STREET & MERRI CREEK
BRIDGE, NORTH FITZROY.

Length of Route, 3½ miles. Time between termini, 28 min.

YELLOW BAND by Day. **CLEAR LIGHT** at Night.

First Car leaves North Fitzroy—Week Days at 5-40 a.m.;
Sundays at 1-30 p.m.

" " Spencer Street—Week Days at 6-8 a.m.;
Sundays at 2 p.m.

Last Car leaves North Fitzroy—Week Days at 11-5 p.m.;
Saturdays at 11-35 p.m.; Sundays at
10-5 p.m.

" " Spencer Street—Week Days at 11-35 p.m.;
Saturdays at 12-5 a.m.; Sundays at
10-35 p.m.

PLACES OF INTEREST ON THE ROUTE:

This route follows the preceding one (No. 2) to the
Fire Brigade's Station at Victoria Parade, and thence
via Brunswick Street to North Fitzroy, passing near
The Fitzroy Town Hall.

The Fitzroy Post Office.

The Fitzroy Cricket and Football Grounds.

The Edinburgh Gardens.

The Merri Creek Bridge leading to
The "Home" of The Little Sisters of the Poor.

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and Prahran,
St. Kilda or Toorak.

At Johnston Street—To Carlton and Abbotsford Line.

SEE RAMBLES FROM THIS ROUTE.

*All Trams Pass the Door or within Two
Minutes Walk of*

George & George's

Federal Emporium.

VISITORS TO MELBOURNE CAN PROCURE ANY
ARTICLE THEY REQUIRE AT THE LOWEST
CASH PRICE IN MELBOURNE.

DRAPERY
GROCERY
FURNITURE
IRONMONGERY
GENT'S TAILORING
BOY'S CLOTHING
BOOTS AND SHOES
TRAVELLING OUTFITS
&c., &c.

GEORGE & GEORGE

LIMITED,

FEDERAL EMPORIUM,
COLLINS STREET.

All Trams pass the Door or within two minutes walk.
Ask Conductor the nearest place to alight.

ROUTE No. 4.—COLLINGWOOD AND CLIFTON HILL CABLE LINE.

TERMINI:—SPENCER STREET & NORTHCOTE BRIDGE.

Length of Route, 3½ miles. Time between termini, 29 min.

RED BAND by Day. RED LIGHT at Night.

First Car leaves Northcote Bridge—Week Days at 5-40 a.m.;
Sundays at 1-30 p.m.

" " " Spencer Street—Week Days at 6-9 a.m.;
Sundays at 2 p.m.

Last Car leaves Northcote Bridge—Week Days at 11 p.m.;
Saturdays at 11-30 p.m.; Sundays at 10 p.m.

" " " Spencer Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-35 p.m.

PLACES OF INTEREST ON THE ROUTE:

Railway Station, Spencer Street.

Menzies Hotel. Goldsbrough's Wool Stores.

Jewish Synagogue. St. Patrick's Hall.

Bourke Street Horse Bazaars and Retail Shops.

General Post and Telegraph Offices.

Royal Arcade. Largest Drapery Shops.

Theatre Royal. Opera House. Bijou Theatre.

Cyclorama. Alexandra Theatre.

Eastern Market. Y.M.C.A. Building.

Houses of Parliament.

Princess Theatre. Bourke and Wills Monument.

Carlton Gardens. Exhibition Building and Aquarium.

Smith Street Shops. Gasworks.

Heidelberg Road.

Old Colonists' Home. Old Actors' Home.

Northcote Bridge and Merri Creek.

*Connection made at Northcote Bridge with the Northcote to
Preston Cable Tramway, at separate fares.*

TRANSFERS ALLOWED

At Johnston Street—To Carlton and Abbotsford Line.

At Swanston Street—Between Spencer Street and Prabran, St.
Kilda or Toorak.

SEE RAMBLES FROM THIS ROUTE.

Paradise . Restores the Liver
Burgundy to its normal con-
dition and CURES
DYSPEPSIA.

Paradise . is the PURE JUICE
Burgundy of the FINEST RUBY
GRAPES. Contains
Iron, it has a smooth
velvety, pleasant flavour, and is a Perfect,
Well-matured Old Wine.

LADIES drinking one glass daily
acquire that pink and
white complexion which has made British
women famed all the world over for their
natural beauty.

15s. per doz. large Bottles (worth 30s.)

AUSTRALIAN
BURGUNDY WINE COMPANY,
281 FLINDERS LANE,
MELBOURNE.

ROUTE No. 5.—NICHOLSON STREET CABLE LINE.

TERMINI:—SPENCER STREET AND PARK STREET,
NORTH FITZROY.

Length of Route, 3½ miles. Time between Termini, 23 min.

BLUE BAND By Day. BLUE LIGHT at Night.

First Car leaves Park Street:—Week Days at 5-50 a.m.; Sundays
at 1-35 p.m.

" " " Spencer Street—Week Days at 6-12 a.m.;
Sundays at 2 p.m.

Last Car leaves Park Street—Week Days at 11-5 p.m.; Saturdays
at 11-35 p.m.; Sundays at 10-7 p.m.

" " " Spencer Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-32 p.m.

PLACES OF INTEREST ON THE ROUTE :

This route follows the preceding one (No. 4) to the
Carlton gardens at Gertrude Street, and thence *via*
Nicholson Street to North Fitzroy, passing

The Exhibition Building and Aquarium.

The Convent of Mercy.

Salvation Army Receiving Home.

Jubilee Skating Rink.

North Fitzroy Railway Station.

Tramway Company's Machine Shop and Car Works.

Park Street Terminus.

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and Prahran,
St. Kilda or Toorak.

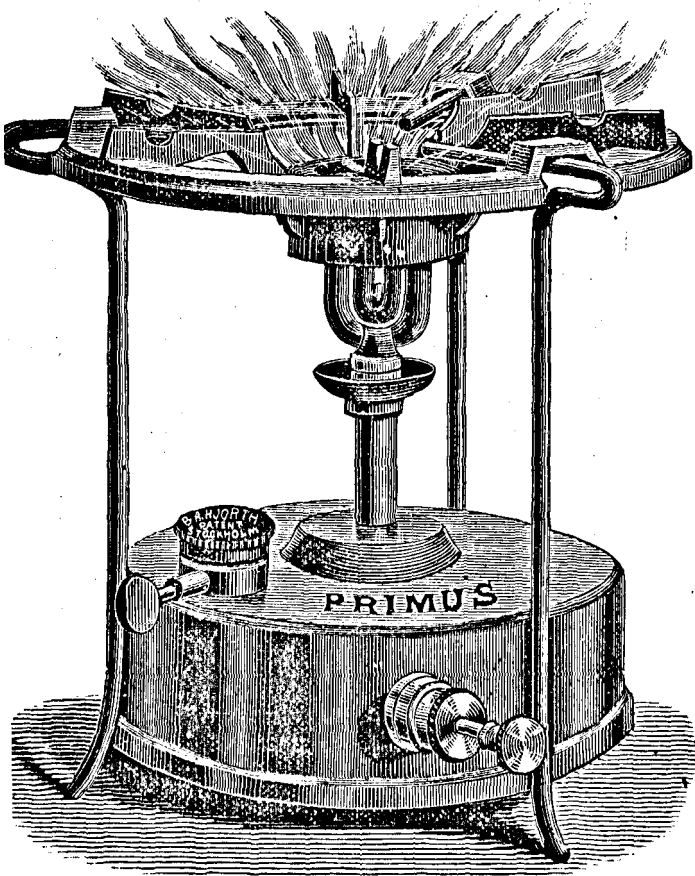
At Johnston Street—To Carlton and Abbotsford Line.

SEE RAMBLES FROM THIS ROUTE.

THE PRIMUS KEROSENE STOVE.

AGENTS:

CHAMBERS & SEYMOUR.



MANUFACTURED IN SWEDEN.

THIS Stove is of entirely novel construction, turning Kerosene Oil into Gas; which burns with an intensely hot blue flame. For this reason it has no equal for

EASY, CLEANLY AND ECONOMICAL COOKING.

BURNS WITHOUT A WICK.

NO SMOKE, SOOT OR SMELL.

COSTS ABOUT ONE FARTHING AN HOUR.

BOILS A QUART OF WATER IN 3 MINUTES.

WILL

BOIL, STEW, FRY, GRILL, TOAST,

AND

HEAT FLAT IRONS.

MADE OF SOLID BRASS THROUGHOUT.

ABSOLUTELY SAFE IN USE.

The Tank is made of HIGHLY FINISHED SOLID BRASS. Both Material and Workmanship being of the VERY BEST.

To be seen working at the Warehouse of

CHAMBERS & SEYMOUR,

Corner Collins & Swanston Sts.,

MELBOURNE.

ROUTE No. 6.—CARLTON AND ABBOTSFORD CABLE LINE.

TERMINI:—FLINDERS ST. & JOHNSTON ST. BRIDGE.

Length of Route, 3½ miles. Time between termini, 28 min.

RED BAND by Day. RED LIGHT at Night.

First Car leaves Johnston Street Bridge—Week Days at 5-35 a.m.; Sundays at 1-30 p.m.

" " " Flinders Street—Week Days at 6 a.m.; Sundays at 2 p.m.

Last Car leaves Johnston Street Bridge—Week Days at 11-10 p.m.; Saturdays at 11-40 p.m.; Sundays at 10-10 p.m.

" " " Flinders Street—Week Days at 11-35 p.m.; Saturdays at 12-5 a.m.; Sundays at 10-40 p.m.

PLACES OF INTEREST ON THE ROUTE :

Princes Bridge and River Yarra.

Hobsons Bay and Princes Bridge Railway Stations.

Church of England Cathedral.

Swanston Street Shops, etc.

Melbourne Town Hall.

Melbourne Hospital.

Russell Street Police Offices, Barracks and Court.

Melbourne Gaol. Trades Hall.

Royal and Horticultural Societies Halls.

Lygon and Elgin Streets.

The Fitzroy Market. The Fitzroy Post Office.

Collingwood Town Hall.

Heidelberg Railway Terminus.

Convent of the Good Shepherd.

Johnston Street Bridge and River Yarra.

Yarra Bend Lunatic Asylum.

Studley Park.

The Cars of this Line run through to Prahran at separate fare, South of Lonsdale Street.

TRANSFERS ALLOWED

At Johnston Street—To the Clifton Hill, Fitzroy, Nicholson Street and North Carlton Lines.

At Lonsdale Street—To Queensberry Street by Toorak Line.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 7.—NORTH CARLTON CABLE LINE.

TERMINI:—FLINDERS ST. & PARK ST., NTH. CARLTON.

Length of Route 2½ miles. Time between termini, 21 min.

GREEN BAND by Day. GREEN LIGHT at Night.

First Car leaves Park Street—Week Days at 5-40 a.m.; Sundays at 1-38 p.m.

“ “ “ Flinders Street—Week Days at 6 a.m.; Sundays at 2 p.m.

Last Car leaves Park Street—Week Days at 11-15 p.m.; Saturdays at 11-45 p.m.; Sundays at 10-15 p.m.

“ “ “ Flinders Street—Week Days at 11-35 p.m.; Saturdays at 12-5 a.m.; Sundays at 10-40 p.m.

PLACES OF INTEREST ON THE ROUTE:

This Route follows the preceding one (No. 6) to Junction of Elgin and Rathdown Streets, and thence *via* Rathdown Street to the terminus, Park Street, North Carlton.

The Cars of this Route run through to St. Kilda at a separate fare, South of Lonsdale Street.

Connection made at Park Street, with East Brunswick Omnibus Line, without extra fare.

TRANSFERS ALLOWED

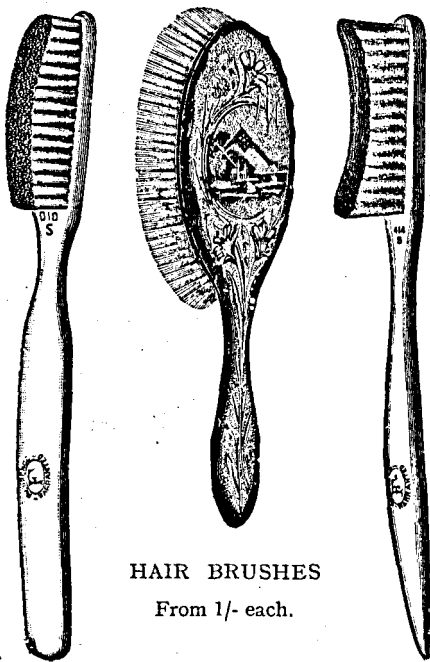
At Johnston Street—By Carlton line to Abbotsford.

At Lonsdale Street—By Toorak line to Queensberry Street.

SEE RAMBLES FROM THIS ROUTE.

TOILET REQUISITES

OF EVERY DESCRIPTION.



HAIR BRUSHES

From 1/- each.

GEORGE & GEORGE'S
FEDERAL EMPORIUM.

All Trams pass the Door or within two minutes walk.

Ask Conductor the nearest place to alight.

ROUTE No. 8.—BRUNSWICK CABLE LINE.

TERMINI:—FLINDERS ST., & MORELAND ROAD.

Length of Route, 4½ miles. Time between termini, 29 min.

RED BAND by Day. RED LIGHT at Night.

First Car leaves Moreland Road—Week Days at 5-35 a.m.;
Sundays at 1-30 p.m.

" " " Flinders Street—Week Days at 6-5 a.m.;
Sundays at 2 p.m.

Last Car leaves Moreland Road—Week Days at 10-57 p.m.;
Saturdays at 11-27 p.m.; Sundays at
10-3 p.m.

" " " Flinders Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-33 p.m.

PLACES OF INTEREST ON THE ROUTE:

Flinders Street Railway Station.

Elizabeth Street Buildings, Shops, &c.

General Post and Telegraph Offices.

St. Francis R.C. Church.

City Meat and Produce Markets.

Hay Market. Pig, Cattle and Horse Markets.

The University of Melbourne and its affiliated
Colleges: Trinity, Ormond and Queen's.

The University Museum.

Melbourne General Cemetery.

Royal Park. Princes Park.

Zoological Gardens. Brunswick Brick Works.

Moreland Road Terminus.

Pentridge Penal Establishment at Coburg.

*Connection made at Moreland Road terminus, with Coburg
Horse Line at separate fares.*

TRANSFERS ALLOWED

At Royal Park—To Zoological Gardens Horse Line.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 9.—NORTH MELBOURNE CABLE LINE.

TERMINI:—FLINDERS ST. & FLEMINGTON BRIDGE.

Length of Route, 3 miles. Time between termini, 22 min.

GREEN BAND by Day. **GREEN LIGHT** at Night.

First Car leaves Flemington Bridge—Week Days at 5-45 a.m.;
Sundays at 1-40 p.m.

" " " Flinders Street—Week Days at 6-7 a.m.;
Sundays at 2-5 p.m.

Last Car leaves Flemington Bridge—Week Days at 11-5 p.m.;
Saturdays at 11-35 p.m.; Sundays at 10-10 p.m.

" " " Flinders Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-35 p.m.

PLACES OF INTEREST ON THE ROUTE:

Flinders Street Railway Station.

Elizabeth Street Buildings, Shops, &c.

General Post and Telegraph Offices.

St. Francis R. C. Church.

City Meat and Produce Markets.

Benevolent Asylum.

North Melbourne Town Hall.

Royal Park.

Flemington Road, Bridge and Borough.

Flemington Racecourse one mile from Terminus.

*Connection made at Flemington Bridge with Moonee
Ponds Omnibus Line. Through Fare, 4d.*

TRANSFERS ALLOWED

At Queensberry Street—To West Melbourne Line.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 10.—WEST MELBOURNE CABLE LINE.

TERMINI:—FLINDERS ST. & QUEENSBERRY ST.

Length of Route, 2½ miles. Time between termini, 15 min.
BROWN BAND by Day. CLEAR LIGHT at Night.

First Car leaves Queensberry Street—Week Days at 6-15 a.m.;
Sundays at 1-45 p.m.

" " " Flinders Street—Week Days at 6-35 a.m.;
Sundays at 2-5 p.m.

Last Car leaves Queensberry Street—Week Days at 11-10 p.m.;
Saturdays at 11-40 p.m.; Sundays at
10-15 p.m.

" " " Flinders Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-35 p.m.

PLACES OF INTEREST ON THE ROUTE:

Flinders Street Railway Station.

Elizabeth Street Buildings, Shops, &c.

General Post and Telegraph Offices.

St. Francis R.C. Church.

The Registrar General's and Lands Titles Offices.

The Patents Office. The Crown Law Offices.

The New Law Courts, including The Supreme, The
County, and The Insolvency Courts.

The Mint. The Old Cemetery.

The Flagstaff Gardens.

The Melbourne Benevolent Asylum.

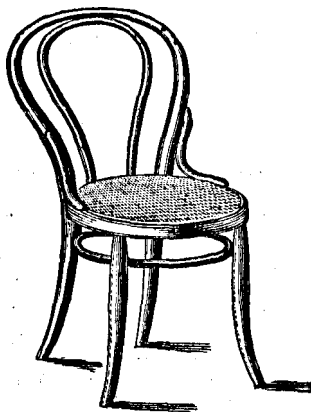
*Connection made at Queensberry Street with North
Melbourne Cable Line, and transfers made without extra
charge.*

TRANSFERS ALLOWED

At Queensberry Street—To North Melbourne Line.

SEE RAMBLES FROM THIS ROUTE.

PARTIES FURNISHING



VISIT
GEORGE & GEORGE'S
FEDERAL EMPORIUM.

FURNITURE, BEDSTEADS, AND CARPETS
AT LESS THAN WHOLESALE PRICES.

All Trams pass the Door or within two minutes walk.

Ask Conductor the nearest place to alight.

ROUTE No. 11.—PRAHRAN CABLE LINE.

TERMINI:—LONSDALE ST. & BALACLAVA ROAD.

Length of Route, 5 miles. Time between termini, 38 min.

RED BAND by Day. RED LIGHT at Night.

First Car leaves Balaclava—Week Days at 5-30 a.m.; Sundays at 1-30 p.m.

" " " Lonsdale Street—Week Days at 5-55 a.m.; Sundays at 1-50 p.m.

Last Car leaves Balaclava—Week Days at 11 p.m.; Saturdays at 11-30 p.m.; Sundays at 10-5 p.m.

" " " Lonsdale Street—Week Days at 11-30 p.m.; Saturdays at 12 p.m.; Sundays at 10-30 p.m.

PLACES OF INTEREST ON THE ROUTE:

Melbourne Hospital. Melbourne Town Hall.

Church of England Cathedral.

Hobsons Bay and Princes Bridge Railway Stations.

Princes Bridge and River Yarra.

Boatsheds and Riverside Path to Botanical Gardens.

Immigrants' Home. Homœopathic Hospital.

Government House Domain.

Military Offices and Barracks.

Church of England Grammar School.

Botanical Gardens. Fawkner Park.

South Yarra and Windsor Railway Stations.

Chapel Street Shops, &c.

Prahran Town Hall.

East St. Kilda and Balaclava.

Connection made at Chapel Street with High Street Omnibus Line to Malvern at SEPARATE FARE, and Dandenong Road Omnibus Line at Through Fare of 4d.

TRANSFERS ALLOWED.

At Lonsdale Street—To Toorak Line for Queensberry Street.

At Flinders, Collins or Bourke Streets—To Spencer Street.

At Chapel Street—To Toorak.

At Wellington Street—To St. Kilda Beach and Esplanade Line.

SEE RAMBLES FROM THIS ROUTE.

GEORGE

—AND—

GEORGE

LIMITED,

The Universal Providers

OF THE

FEDERAL EMPORIUM,

*Are the Best Company in Melbourne for
Country Visitors to do Business with.*

Reliable Goods only are kept in Stock and
the Prices are the Lowest possible for
Cash.

ADDRESS—

162, 164, 166, 168 Collins Street }
193, 195, 197, 199 Little Collins St., } MELBOURNE.

All Trams pass the Door or within two minutes walk.
Ask Conductor the nearest place to alight.

ROUTE No. 12.—TOORAK CABLE LINE.

TERMINI:—QUEENSBERRY ST. & IRVING RD., TOORAK.

Length of Route, 5 miles. Time between termini, 36 min.

WHITE BAND by Day. CLEAR LIGHT at Night.

First Car leaves Toorak—Week Days at 7-30 a.m.; Sundays at 1-30 p.m.

" " " Queensberry Street—Week Days at 8 a.m.; Sundays at 2-5 p.m.

Last Car leaves Toorak—Week Days at 10-50 p.m.; Saturdays at 11-20 p.m.; Sundays at 9-50 p.m.

" " " Queensberry Street—Week Days at 11-30 p.m.; Saturdays at 12 p.m.; Sundays at 10-30 p.m.

PLACES OF INTEREST ON THE ROUTE:

City Public Baths. Hibernian Hall.

Workingmen's College.

Public Library, Museum and Art Gallery.

Oddfellows Hall.

Melbourne Hospital.

This route then follows the preceding one (No. 11) to Chapel Street, South Yarra, and thence up Toorak Hill, passing numerous handsome residences to Irving Road Terminus.

TRANSFERS ALLOWED

At Lonsdale Street—To Prahran or St Kilda Lines.

At Flinders, Collins or Bourke Streets—To Spencer Street.

At Chapel Street—To Prahran, Windsor and Balaclava.

SEE RAMBLES FROM THIS ROUTE.

STYLE AND ECONOMY SECURED AT



George & George's Federal Emporium.

The Leading Mantle House in the City.

ROUTE No. 13.—ST. KILDA CABLE LINE.

TERMINI:—LONSDALE ST. & BRIGHTON ROAD.

Length of Route, 4½ miles. Time between termini, 30 min.

GREEN BAND by Day. GREEN LIGHT at Night.

First Car leaves Brighton Road—Week Days at 5-35 a.m.;
Sundays at 1-35 p.m.

“ “ “ Lonsdale Street—Week Days at 5-55 a.m.;
Sundays at 1-50 p.m.

Last Car leaves Brighton Road—Week Days at 11-10 p.m.;
Saturdays at 11-40 p.m.; Sundays at
10-15 p.m.

“ “ “ Lonsdale Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-30 p.m.

PLACES OF INTEREST ON THE ROUTE:

This Route follows that of the Prahran line (No. 11)
to the junction of Domain and St. Kilda Roads,
and thence *via* St. Kilda Road, passing

The Church of England Grammar School.
Albert Park. Fawkner Park.

Alfred Hospital. Wesley College.
Asylum for Deaf and Dumb. Asylum for the Blind.
Warehouseman's Cricket Ground.

Jewish Almshouses.

St. Kilda Town Hall.

Brighton Road.

TRANSFERS ALLOWED

At Lonsdale Street—To Toorak Line for Queensberry Street.

At Flinders, Collins or Bourke Streets—To Spencer Street.

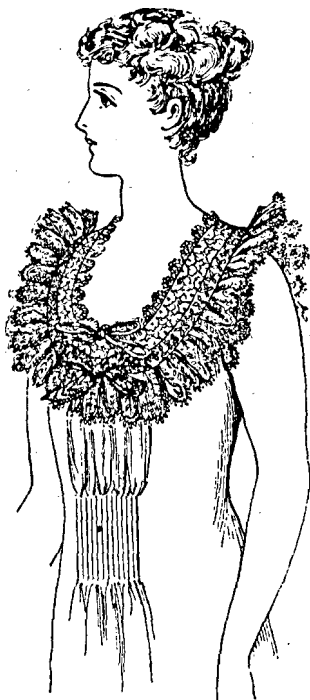
At St Kilda Junction—To Esplanade and Beach Line.

SEE RAMBLES FROM THIS ROUTE.

George & George Ltd.,

MANUFACTURERS OF

LADIES UNDERCLOTHING.



Every Description of Ladies Underclothing made on the
Premises.

BRIDAL TROUSSEAUX AND LAYETTES.

Prices the Lowest in the City.

**Federal Emporium } COLLINS STREET,
LITTLE COLLINS STREET.**

ROUTE No. 14.—ST. KILDA ESPLANADE CABLE LINE.

TERMINI:—CHAPEL ST. WINDSOR & ACLAND ST.
ST. KILDA.

Length of Route, 2 miles. Time between termini, 12 min.

WHITE BAND by Day. CLEAR LIGHT at Night.

First Car leaves Acland Street—Week Days at 7-15 a.m.;
Sundays at 1-30 p.m.

" " " Chapel Street—Week Days at 7-30 a.m.;
Sundays at 1-45 p.m.

Last Car leaves Acland Street—Week Days at 11-40 p.m.;
Saturdays at 12-10 a.m.; Sundays at
10-40 p.m.

" " " Chapel Street—Week Days at 11-55 p.m.;
Saturdays at 12-25 a.m.; Sundays at
10-55 p.m.

*NOTE.—In Summer (between November 1st and May
1st) the line starts 1½ hours earlier on week days.*

PLACES OF INTEREST ON THE ROUTE:

Dandenong Road.

All Saints' Church of England.

St. Kilda Junction. Albert Park.

Cricket, Football, and Cycling Grounds.

St. Kilda Railway Station.

The Sea Baths. The Pier.

The St. Kilda Esplanade and Sea Beach.

*Connection made with the Dandenong Road Omnibus
Line. Through Fare, 4d.*

TRANSFERS ALLOWED

At St Kilda Junction—To St. Kilda Line.

At Chapel Street—To Prahran and Balaclava Line.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 15.—SOUTH MELBOURNE CABLE LINE.

TERMINI:—GISBORNE ST. & BEACONSFIELD
PARADE & BEACH.

Length of Route, 2 $\frac{3}{4}$ miles. Time between termini, 26 min.
GREEN BAND by Day. GREEN LIGHT at Night.

First Car leaves Beach—Week Days at 5-45 a.m.; Sundays
at 1-40 p.m.

" " Gisborne Street—Week Days at 6-10 a.m.;
Sundays at 2-5 p.m.

Last Car leaves Beach—Week Days at 11 p.m.; Saturdays
at 11-30 p.m.; Sundays at 10 p.m.

" " Gisborne Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at
10-30 p.m.

PLACES OF INTEREST ON THE ROUTE :

The Gordon Monument.

Parliament Houses and Gardens.

The Treasury and Government Offices.

The Melbourne Club. The Masonic Hall.

The Doctors' Quarter.

The Congregational "Scots" and Baptist Churches.

The Newspaper Offices. The Athenæum Hall, &c.

Melbourne Town Hall.

The "Block" Arcade. Banks and Offices.

The Savings Bank. The Customs House.

The Queen's Bridge. The River and Wharves.

The City Road Breweries and Manufactories.

Clarendon Street Shops, &c.

The South Melbourne Town Hall.

Beaconsfield Parade and Sea Beach.

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and Prahran,
St. Kilda or Toorak.

At Market Street—To Richmond Line for Spencer Street or
Swanston Street.

At Clarendon Street—To Port Melbourne.

SEE RAMBLES FROM THIS ROUTE

ROUTE No. 16.—PORT MELBOURNE CABLE LINE.

TERMINI:—GISBORNE STREET AND PORT
MELBOURNE RAILWAY STATION.

Length of Route, 2½ miles. Time between termini, 24 min.

WHITE BAND by Day. ORANGE LIGHT at Night..

First Car leaves Port Melbourne—Week Days at 5-50 a.m.;
Sundays at 1-40 p.m.

" " " Gisborne Street—Week Days at 6-10 a.m.;
Sundays at 2-5 p.m.

Last Car leaves Port Melbourne—Week Days at 11-5 p.m.;
Saturdays at 11-35 p.m.; Sundays at 10-5 a.m.

" " " Gisborne Street—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-30 p.m.

PLACES OF INTEREST ON THE ROUTE :

This Route follows that of the preceding Line (No. 15), to junction of City Road and Clarendon Street, and thence *via* City Road and Bay and Beach Streets, passing Port Melbourne Town Hall to Port Melbourne Piers, Beach, and Railway Station.

TRANSFERS ALLOWED

At Swanston Street—Between Spencer Street and Prahran, St. Kilda or Toorak.

At Market Street—To Richmond Line for Spencer or Swanston Street.

At Clarendon Street—To South Melbourne.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 17.—KEW HORSE LINE.

TERMINI:—VICTORIA BRIDGE & KEW CEMETERY.

Length of Route, 1½ miles. Time between termini, 15 min.

LETTERED BAND by Day. CLEAR LIGHT at Night.

First Car leaves Kew—Week Days at 8 a.m.; Sundays at 1-30 p.m.

“ “ “ Victoria Bridge—Week Days at 8-20 a.m.;
Sundays at 1-50 p.m.

Last Car leaves Kew—Week Days at 11-20 p.m.; Saturdays at
11-20 p.m.; Sundays at 10 p.m.

“ “ “ Victoria Bridge—Week Days at 11-40 p.m.;
Saturdays at 11-40 p.m.; Sundays at
10-20 p.m.

PLACES OF INTEREST ON THE ROUTE :

Victoria Bridge.

River Yarra.

Kew Township.

Kew Railway Station.

Kew Post Office.

The Kew Cemetery.

*Connection made at Victoria Bridge with the Victoria
Street Cable Line. Through Fare, 4d.*

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 18.—HAWTHORN HORSE LINE.

TERMINI:—HAWTHORN BRIDGE, RICHMOND &
AUBURN ROAD.

Length of Route, 2 miles. Time between termini, 19 min.
LETTERED BAND by Day. CLEAR LIGHT at Night.

First Car leaves Auburn Road—Week Days at 8 a.m.; Sundays
at 1-30 p.m.

" " " Hawthorn Bridge—Week Days at 8-20 a.m.;
Sundays at 1-55 p.m.

Last Car leaves Auburn Road—Week Days at 11-30 p.m.;
Saturdays at 12 p.m.; Sundays at 10-35 p.m.

" " " Hawthorn Bridge—Week Days at 11-55 p.m.;
Saturdays at 12-25 a.m.; Sundays at
10-55 p.m.

PLACES OF INTEREST ON THE ROUTE:

Hawthorn Bridge.

River Yarra and Boat Sheds.

St. James' Park.

Hawthorn Railway Station.

Hawthorn Township.

Swan Street Bridge.

Riversdale Road.

Auburn.

*Connection made at Hawthorn Bridge with the Richmond
Cable Line. Through Fare, 4d.*

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 19.—ROYAL PARK HORSE LINE.

TERMINI:—ROYAL PARK ENTRANCE & ZOOLOGICAL
GARDENS.

Length of Route, $\frac{3}{4}$ of a mile. Time between termini, 5 min.

WHITE BAND by Day. CLEAR LIGHT at NIGHT.

First Car on Week Days at 10 a.m.; Sundays at 2-5 p.m.

Last Car on week days at time of closing the Zoological Gardens.

Varying from 4-45 p.m. in winter, to 6 p.m. in summer.

This Line runs entirely within the Royal Park, passing by the stone commemorative of the starting point of the Burke and Wills exploring expedition across Australia and the paddocks of the Acclimatisation Society, to the Zoological Gardens.

TRANSFERS ALLOWED

At Royal Park entrance—To Brunswick Cable Line.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 20.—PRAHRAN AND MALVERN OMNIBUS LINE (via High Street).

TERMINI:—PRAHRAN AND MALVERN TOWN HALLS.

Length of Route, 2 miles. Time between termini, 20 min.
LETTERED BAND by Day. CLEAR LIGHT at Night.

First Bus leaves Prahran—Week Days at 8 a.m.; Sundays at
1-30 p.m.

" " " Malvern—Week Days at 8-25 a.m.; Sundays
at 1-55 p.m.

Last Bus leaves Prahran—Week Days at 11 p.m.; Saturdays at
11 p.m.; Sundays at 10-30 p.m.

" " " Malvern—Week Days at 11-30 p.m.; Saturdays
at 11-30 p.m.; Sundays at 11 p.m.

This Line connects Prahran *via* High Street with the Districts of Mount Erica, Armadale, and Malvern, passing The Avenue, Williams Road, Orrong Road, Armadale Railway Station and Boundary Road to the Glenferrie Road, not far from the Malvern Railway Station.

*Connection made at Chapel Street with the Prahran Cable
Tramway Line at Separate Fares.*

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 21.—DANDENONG ROAD OMNIBUS LINE.

TERMINI:—CHAPEL ST. AND WATTLETREE ROAD.

Length of Route, 1½ miles. Time between termini, 12 min.

LETTERED BAND by Day. CLEAR LIGHT at Night.

First Bus leaves Chapel Street—Week Days at 8 a.m.; Sundays
at 2 p.m.

" " " Wattletree Road—Week Days at 8-15 a.m.;
Sundays at 2-15 p.m.

Last Bus leaves Chapel Street—Week Days at 11-15 p.m.;
Sundays at 10-30 p.m.

" " " Wattletree Road—Week Days at 11-30 p.m.;
Sundays at 10-45 p.m.

This Line connects Windsor and Prahran with East St. Kilda and Dandenong Road districts, passing the St. Kilda Cemetery, Westbury Street, and the Avenue, Williams and Orrong Roads to the Wattletree Road at Armadale.

*Connection made at Chapel Street with the Prahran and
St. Kilda Esplanade Cable Tramway Lines, at
Through Fare of 4d.*

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 22.—MOONEE PONDS OMNIBUS LINE.

TERMINI: — FLEMINGTON ROAD, NORTH MELBOURNE, AND THE JUNCTION, MOONEE PONDS.

Length of Route, 2 miles. Time between termini, 17 min.

LETTERED BAND by Day. CLEAR LIGHT at Night.

First Bus leaves Moonee Ponds—Week Days at 8 a.m.; Sundays at 1-30 p.m.

" " " North Melbourne—Week Days at 8-20 a.m.; Sundays at 1-50 p.m.

Last Bus leaves Moonee Ponds—Week Days at 11-35 p.m.; Saturdays at 12-5 a.m.; Sundays at 10-40 p.m.

" " " North Melbourne—Week Days at 11-55 p.m.; Saturdays at 12-25 a.m.; Sundays at 11 p.m.

This Line connects North Melbourne with the districts of Flemington, Ascot Vale and Moonee Ponds, *via* the Mount Alexander Road, and has its terminus near to

The Essendon Town Hall and
The Moonee Valley Race Course.

Connection made at Flemington Road with the North Melbourne Cable Tramway Line at Through Fare of 4d.

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 23.—EAST BRUNSWICK OMNIBUS LINE.

**TERMINI:—RATHDOWN ST., NORTH CARLTON, AND
ALBERT ST., EAST BRUNSWICK.**

Length of Route, 1 mile. Time between termini, 7 min.

LETTERED BAND by Day. CLEAR LIGHT at Night.

**First Bus leaves East Brunswick—Week Days at 7-20 a.m.;
Sundays at 1-20 p.m.**

**" " " North Carlton—Week Days at 7-30 a.m.;
Sundays at 1-30 p.m.**

**Last Bus leaves East Brunswick—Week Days at 11-50 p.m.;
Saturdays at 12-10 a.m.; Sundays at
10-50 p.m.**

**" " " North Carlton—Week Days at 12 p.m.; Satur-
days at 12-20 a.m.; Sundays at 11 p.m.**

*Connection made at Rathdown Street with North Carlton
Cable Tramway Line by Transfer, without extra charge.*

SEE RAMBLES FROM THIS ROUTE.

ROUTE No. 24.—CHURCH STREET OMNIBUS LINE.

TERMINI:—SWAN ST. RICHMOND & TOORAK ROAD,
SOUTH YARRA.

Length of Route, 1 mile. Time between termini, 12 min.

LETTERED BAND by Day. CLEAR LIGHT at Night.

First Bus leaves South Yarra—Week Days at 10-15 a.m.; Sundays
1-45 p.m.

" " " Richmond—Week Days at 10-30 a.m.; Sundays
at 2 p.m.

Last Bus leaves South Yarra—Week Days at 10-30 p.m.; Sun-
days 10-30 p.m.

" " " Richmond—Week Days at 10-45 p.m.; Sundays
at 10-45 p.m.

This Line connects Richmond with South Yarra,
and by transfer to the Prahran and Toorak
Cable Tramway Lines, without extra fare, with
Prahran, Windsor, Balaclava, and Toorak, also by
further transfer at Wellington Street with St. Kilda
Beach and Esplanade.

*This Line does not run during the winter, from May 1st to
October 31st.*

SEE RAMBLES FROM THIS ROUTE.

Fares.

The FARE ON EACH TRAMWAY and OMNIBUS LINE for the whole or any portion of the route (with the exceptions noted below) is 3d.

THE FARES ARE PAYABLE to the CONDUCTOR or DRIVER on demand as under—

ADULTS and CHILDREN over 12 years of age FULL FARE.

CHILDREN over 3 and under 12 years of age HALF FARE.

CHILDREN under 3 years of age, if NOT CARRIED IN LAP, or if MORE THAN ONE CHILD to the Passenger HALF FARE.

CHILDREN under 3 years of age, if CARRIED IN LAP, and NOT MORE THAN ONE CHILD to the Passenger FREE.

The exceptions referred to above are—

BRUNSWICK LINE, for whole or any part of Route 2d.

ROYAL PARK LINE, for whole or any part of Route 2d.

RICHMOND LINE, between Swanston and Spencer Streets, for Adult or Child .. 1d.

CLIFTON HILL LINE, between Northcote Bridge and Johnston Street, for Adult or Child 1d.

SOUTH MELBOURNE LINE, between Clarendon Street and the Beach Terminus, for Adult or Child .. 1d.

All Lines running BETWEEN SPENCER and SPRING STREETS, by CITY TICKET ONLY, 1½d.

All Lines running BETWEEN FLINDERS and VICTORIA STREETS, by CITY TICKET ONLY, 1½d.

West Melbourne Line, BETWEEN FLINDERS AND SPENCER STREETS, by CITY TICKET ONLY .. 1½d.

Luggage Rates.

Personal Luggage under 28lbs. in weight and not exceeding 18 inches square in bulk .. FREE.

Luggage or Packages exceeding above-named weight or bulk, for each article 3d.

All luggage too large to go under the car seats must be placed on the platform of the car.

Passengers must look after the safety of their luggage and parcels, as the Company accepts NO RESPONSIBILITY for them. It reserves the right for its officers to refuse any they may think objectionable to passengers or likely to injure the car or its furniture.

No luggage, letters, or parcels of any kind will be carried by the Company unless accompanied by a passenger.

Payment for luggage, &c., must be registered by the Conductor with his bell punch in the same way as passengers' fares.

Tramway Tickets.

To be obtained from Conductors or Drivers, and at Company's Offices.

GENERAL TICKETS—Good on any line,
Sold in packets of 12 for 2s. 9d.

CITY TICKETS—Good on any line between Spencer and Spring Streets, or between Flinders and Victoria Streets, but NOT AVAILABLE ON SUNDAYS OR HOLIDAYS, and good for children at full fare only.
Sold in packets of 8 for 1s.

COUPON TICKETS—Good for through ride from Hawthorn or Kew to Melbourne *via* Richmond or Victoria Street Cable Lines respectively. (Obtainable from Drivers of Horse Line.) Sold by Single Ticket at 4d. each.

BEACH RETURN TICKETS—Good for through journey from any of the Suburbs North of the Yarra to the Beach at St. Kilda or South Melbourne and return.
Sold in sets of four Tickets, one for each car used during the trip, at 9d. for Adults.
4½d. for Children.

SCHOOL TICKETS—Good for Children under 17 years of age, on school days and between 8 a.m. and 6 p.m. Sold only through Teachers of Schools. See Regulations, Page 48.

Regulations for the Sale and Use of School Tramway Tickets.

School Tickets will **only be issued during the first week** of each School Quarter, and only for the use of pupils under 17 years of age, when riding to or from School between 8 a.m. and 6 p.m. on School Days, and will **not be good** for use on Saturdays, Sundays, Public Holidays, and during School Vacations.

The Tickets will be sold at 9s. per 100.

They will be fastened in books of 10 each and sold only in one or more lots of 100 Tickets each for the use of any one pupil.

The names and addresses of children for whom Tickets are bought will be registered by the Company, and entered together with the name of school on the cover of each book of Tickets issued.

The book of Tickets must be handed to the Conductor when the fare is to be paid, and **no Ticket will be accepted unless detached** from the book by the Conductor, who will then return the book and remainder of Tickets to the passenger.

School Tickets will be sold only through the Principal of the school which the pupil attends, and upon a requisition furnished by him stating the following particulars, the correctness of which must be certified by him :—

Name, age, and residence of pupil.

Tramway Line or Lines Tickets are to be used upon.

Number of Tickets required (which must be 100 or some multiple thereof).

Such requisition must be forwarded to the Company's Office, addressed to the Manager, before or during the first week of each School Quarter, together with the amount to be paid for the Tickets.

W. G. SPRIGG, SECRETARY.

Transfer Tickets.

TRANSFER Tickets are issued by Conductors to passengers entitled to transfer from one line to another, at the places stated under the heading of each Route in this Guide.

TRANSFER TICKETS ARE GOOD ONLY ON the day of issue, and until the time and for the destination marked by the Conductor. They are intended to give a CONTINUOUS RIDE ONLY, and passengers holding them should take the next car passing for their destination. If offered for a different destination or later than the time marked, the tickets will be refused and ANOTHER FARE MUST BE PAID.

When receiving Transfer Tickets passengers should examine them and see that they are properly marked by the Conductor, for errors must be corrected at once, and cannot afterwards be rectified.

Transfer Tickets are issued daily, except those between Spencer Street and Prahran, St. Kilda or Toorak, which are not available on Sundays and Holidays.

Check Tickets.

ON lines where less than the full fare 3d. is accepted for a section of the line only, Check Tickets are issued by Conductors to passengers who pay a full fare, to show that the passenger is entitled to ride beyond the section without further payment.

They are intended for the protection of the passenger against any further demand by the Conductor, and must be delivered to him should the passenger leave the car before or when the end of the section is reached, or at the next collection of fares after the car passes such section.

If not returned to the Conductor when demanded, another fare must be paid.

Lost Articles.

ARTICLES left in cars by passengers are, if seen by Conductors, taken to the local office of the line and thence forwarded the next morning to the Head Office, Bourke Street West.

Articles that are found can be obtained on application and identification at the Head Office. If not claimed within two weeks such articles are sent to the Town Clerk's Office, Melbourne Town Hall, where application thereafter must be made.

The Company ACCEPTS NO RESPONSIBILITY whatever for articles left in the cars.

Complaints and Reports by Passengers and Others.

IT is desired that passengers and others who do not receive proper treatment from the Company's employees, or who think the latter do not perform their duties properly, will report such matters to the Manager, at the Head Office, Bourke Street, either personally or by letter.

In making reports it is necessary to give either the number of the car or of the employee's badge, with the date, time and place of the occurrence, and full particulars. Without this information the employee complained of cannot always be identified.

All reports are treated as *strictly confidential*, and hence passengers and others need not hesitate to make them through fear of unpleasantness afterwards.

Reports are always carefully investigated and the employee dealt with in accordance with the nature of the offence. Unless misconduct is very serious, employees are not dismissed until they repeat offences for which they have been cautioned.

Cautions to Passengers.

Wait till the car stops before you get on or off.

If the car is not stopped for you at the right place, or quickly enough, do not be tempted to get off or on while in motion, but wait and avoid danger.

Do not get off or on after the signal to start (two bells) is given.

Always get on or off at Rear Platform of Car, and at side nearest the footpath.

Avoid getting off grip car on side next another car which is approaching or standing alongside, as it is dangerous.

Look both ways before getting off and see that the road is clear and safe.

Never pass from one car to the other while they are in motion.

Never ride on the platform steps, or stand at side of grip car. It is attended with danger.

When riding on Car Platforms, hold on by rail, especially at curves.

When riding on grip car sit squarely and firmly, especially when going around curves.

When riding on side seats of grip car, look out for objects near the track and avoid striking leg or foot.

Never cross the track before an advancing car, but let it pass and then cross.

Never cross the track behind a car till you have looked to see that no other car is coming on other track. Especially remember this when getting off a car and crossing behind it.

Remember that accidents from neglect of any of the above cautions can easily be prevented, and will be entirely your own fault. It is better to lose a little time than run the risk of injury.

Notice to Passengers.

Passengers are requested to HAVE their FARES READY for the Conductor when he collects, and AVOID REQUIRING CHANGE if possible.

CALL the CONDUCTORS ATTENTION TO STOP the Car, as passengers are not allowed to ring the bell.

CARS will ONLY STOP at the FURTHER SIDE of street crossings, at which place passengers should wait. Also at the centre of large blocks.

CARS will NOT STOP ON CURVES, OR when ASCENDING the STEEP GRADES in Collins Street East, and Bourke Street West.

Report cases of inattention, incivility, errors of change, &c., direct to the Manager at Tramway Offices, Bourke Street, in writing, giving date, time, place, number of car or Conductor's badge and full particulars. All complaints receive attention.

Also kindly report all cases where Fares are not Punched, or are not collected. All such reports are treated as confidential.

The rule for registration of Fares is as follows :—

Conductors are required to punch the proper trip slip (attached to his coat) with the Bell Punch once for every full or half fare received in cash, and to Punch each Ticket received separately one by one (except Transfer and Check Tickets, and Employees' Passes).

All Punching should be done immediately the Fare is received, before change is made, and while facing the Passenger.

By-laws and Regulations.

1.—The By-laws and Regulations hereinafter set forth shall extend and apply to all tramcars of the Company, and to all premises with respect to which the Company have power to make By-laws or Regulations.

2.—No passenger shall smoke inside or upon the platforms of any closed-in or box car, or on any car other than the grip-car, used on cable tramway lines.

3.—No passenger or other person, while riding in or upon any car, shall solicit alms, play or perform upon any musical instrument, distribute any handbills or advertisements, or, without having first obtained the permission of the Company in writing, offer for sale any article.

4.—No person who in the opinion of the conductor is in a state of intoxication shall be allowed to enter or mount upon any car; and if found in or upon any car shall, at the request of the conductor, leave the car upon the fare, if previously paid, being returned; and if he do not at once leave the car, he may be removed by or under the direction of the conductor; and in such case he shall also be liable to the penalty provided by these By-laws and Regulations.

5.—No person shall swear or use obscene or offensive language whilst in or upon any car, or commit any nuisance in or upon any car, or wilfully interfere with the comfort of any passenger; and any person who in the opinion of the conductor shall have committed a breach of this Regulation shall leave the car on the request of the conductor to do so upon the fare, if previously paid, being returned; and if he do not leave the car at once, he may be removed by or under the direction of the conductor, and he shall be also liable to the penalty fixed by these By-laws and Regulations.

6.—No person shall in any way soil or damage any car, or remove or interfere with any fastening on any car, or remove, affix, or deface any number, plate, printed notice or advertisement, or writing on any part of, or break or scratch any window of, or otherwise wilfully damage, any car. Any person acting in contravention of this Regulation shall be liable to the penalty prescribed by these By-laws and Regulations, in addition to the liability to pay the amount of any damage done.

7.—No person whose dress or clothing might, in the opinion of the conductor, soil or injure the dress or clothing of any passenger, or who for any other reason might be offensive to passengers, shall be entitled to enter or remain in or upon any car; and any such person may be prevented from entering any car; nor shall he enter any car after being requested not to do so by the conductor; and if found on any such car, he shall on the request of the conductor leave the car upon the fare, if previously paid, being returned; and if he do not at once leave the car he may be removed by or under the direction of the conductor; and in such case he shall also be liable to the penalty provided by these By-laws and Regulations.

8.—Each passenger shall upon demand exhibit and deliver up his ticket, or pay to the conductor the fare legally demandable for the journey.

9.—Personal or other luggage (including the tools of workmen) shall, unless otherwise permitted by the conductor, be placed on the platform, and not in the interior or on the roof of any car.

10.—Every passenger who shall take upon a car any article not being personal luggage, or any personal luggage which shall exceed altogether the weight of 28lb., shall pay to the conductor on demand the sum of 3d. for every such article or for each package of luggage so in excess. Nevertheless, the conductor may refuse any such article or any such excess luggage, and may remove it from the car if found thereon.

11.—No passenger or other person not being a servant of the Company shall enter the grip space of any grip-car, or stand on the front of any grip-car while it is in motion, or travel on the steps of any car, or pass from one car to another while they are in motion.

BY-LAWS AND REGULATIONS.

12.—No child under the age of ten years shall be entitled to ride on the grip-car of any cable tramway unless accompanied by some adult person.

13.—No person except a passenger, or intending passenger, shall enter or mount any car, and no person shall hold or hang on by or to any part of the car.

14.—When a car contains the full number of passengers which it is licensed to carry, no additional person shall enter, mount, or remain in or on any such car when warned by the conductor not to do so.

15.—No person shall travel in or on any car with loaded firearms, paints, oils, or glass, or anything which shall project beyond the car, or which may, in the opinion of the conductor, be liable to damage the car, or the person or property of any person.

16.—No person shall wilfully obstruct or impede any officer or servant of the Company in the execution of his duty upon or in connection with any car or tramway of the Company.

17.—No person shall be entitled to have any dog or other animal in or on any car.

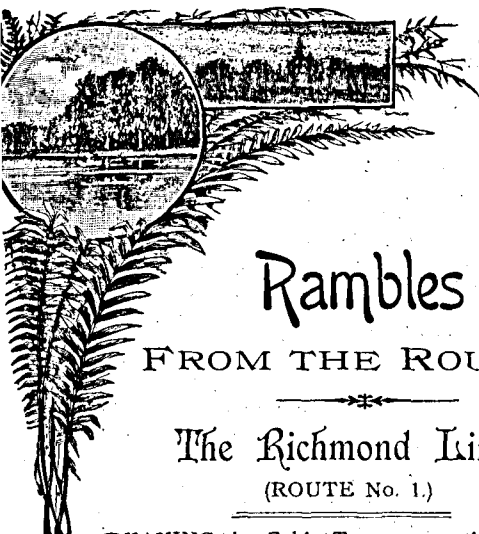
18.—Any person offending against or committing a breach of any of these By-laws and Regulations shall be liable to a penalty not exceeding forty shillings for each offence.

19.—The expression "Conductor" shall include any officer or servant in the employment of the Company.

20.—These By-laws shall come into force on the 1st day of March, 1890.

W. G. SPRIGG, *Secretary.*





Rambles

FROM THE ROUTES.

The Richmond Line.

(ROUTE No. 1.)

LEAVING the Cable Tramway at the bridge, the Horse Tram may be taken, for a penny additional transfer ticket, for a further ride of some two miles to the end of the Hawthorn line, beyond which attractive roads open in all directions.

But before taking the Horse Tram there is an especial ramble by the river side, which may well be taken, and once taken will likely be repeated. To the right of the Cable Tram Terminus and of the bridge is an opening down to the river bank; the post and rail fence on the left offers small obstruction, and is intended as an obstacle to stray cattle and horses only. Passing between the rails we descend to meadow land and to the river bank; the walk along this bank is a well defined one, and improves with every quarter of a mile of its onward course. The Rambler on this route is soon quite "in the country." Passing under two railway bridges the bounds of the old Survey Paddock (now called Richmond Park) are entered upon, and from this point out along the river bank to the left of the pedestrian and on to Church Street bridge at Richmond the ramble is a delightful one. For a Saturday afternoon's outing for a family nothing better could be recommended.

Along the river will be seen the "fours" and "eights" of the rowing clubs at practice, most of

whom show well in their club costumes. The river is here all the way nicely fringed with wattles and willows; half way through this walk will be found on the river bank a neat refreshment and boat house. Here tea, coffee, Australian wines and temperance drinks, fruits and cakes are to be had in rooms commanding a view of the river, and the South Yarra mansions and villas on its opposite bank. A smoke and a quarter of an hour's rest may be advantageously taken here.

The opportunity here offers also of taking a boat down the river to Richmond or Princes Bridge, and it should not be overlooked. Boats to suit requirements can be had, with a boy for steering or rowing, at a small charge. As the course of the river is downwards the matter of rowing becomes very easy.

Should the walk along the river bank be continued it will be found full of interest to the Rambler. The curves of the river are many, and present new views at every turn. The Survey Paddock (as it was called) has always been a favorite place for pic-nics, and here annually for many years the "German pic-nic" of the Turn Verein was held. The Horticultural Society's grounds are situated in this park; these are always open to the inspection of the public, and a call may be well made here as an interlude in the ramble. The bridge of that little used, and last made of our railway lines—the Glen Iris line—is a handsome structure as seen crossing the river midway on this ramble.

The bluestone quarries next form a feature of the river bank, and continue to do so until Church Street Bridge is reached; many of these quarries are disused, and are now transformed into large basins of water; a turn inland to the right for the last mile of the walk will bring the Rambler into Swan Street, if the length of the route to Church Street be not desired. Beyond Church Street the ramble may be continued to Punt Road, or the Omnibus may be taken in summer (see Route No. 24) at Church Street for Toorak Road as for one way, or to Swan Street for another. Altogether an interesting afternoon's ramble can be had on this route, to be varied at the will and taste of the Rambler.

The Victoria Street Line.

(ROUTE No. 2.)

LEAVING the car at its terminus, a walk back on the line for a few yards and a turn to the right there, brings the Rambler from this route to the new Burnley Street bridge—a footbridge only—across the Yarra. From this bridge a look downward to the right may be given to the extensive Chinamen's gardens, to be seen on the other side of the Yarra bank; a visit to these gardens is gratefully received by the toiling Mongolians, though an inspection of their primitively-built habitations is not invited. The gardens and their method of cultivation are matters of novelty to British eyes, and it is not a little that may here be learned by those in anyway acquainted, and who is not, with amateur gardening.

Surmounting the rise on the other side of the river, Studley Park, pre-eminent for pic-nics, and as a Sunday resort, opens on the left; a ramble may be made around its river bank to the queer old Church Street Bridge—the next bridge made over the Yarra after the building of the old Princes Bridge. It is not everyone that is acquainted with this out of the way old wooden structure, to cross which the antiquated custom of a penny toll remains. Retracing one's steps, all Studley Park, on both sides of the Studley Park Road, is at the service of the Rambler, and many are the pretty views to be had from its hilly and most picturesque points. The walk may be continued over to the Johnston Street Bridge, whence the Rambler may return, on crossing that bridge, by the Carlton and Abbotsford line (Route 6). From the Studley Park Road, on return from Church Street Bridge, a good view will be obtained of the Abbotsford Convent and grounds.

If this ramble be not taken, the Kew Horse Tram (Route 17) may be taken for an additional penny

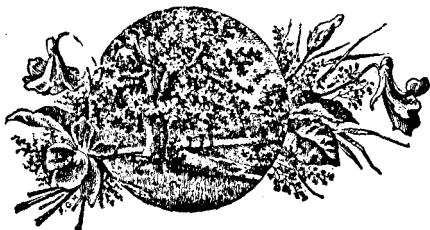
fare, through Kew township, passing on its way the Studley Park Road, the Cotham or Whitehorse Road, and the Doncaster Road. At either of these roads the tram may be left and an interesting ramble begun over well made routes. That of Studley Park Road introduces to one's notice its fine villa buildings on either side. On their termination, to the right of the road a descent may be made to the pretty valley intersected by the Yarra, and the ramble hereabout may be extended for miles. Or for another way (to use Mrs Glasse's cookery book expression) a boat may be taken at the boat house at the foot of this descent from the road. A pleasant row up the river is then before one, between banks well shaded by willow and other trees. A row of an hour or more will well repay those who take it, showing as it does the Yarra in other aspects than can be seen from the bridges about Melbourne.

The Whitehorse road turns to the right, and is that leading to Nunawading and to Ringwood, and its antimony mines. Agriculture is seen in various aspects on both sides, and the village of Ringwood has the appearance of an old English village in many ways, not to mention that of its little wayside inns. The Doncaster Road extends beyond the terminus of the Horse Car Line, and about a mile onwards a turning to the left will lead to the Heidelberg Bridge, whence that pretty and first settled of all suburban townships may be reached. The return from Heidelberg must, however, be made by railway, the station being very convenient to the township. The Doncaster Road takes the Rambler on the way to the famous "Tower" there erected, from which the view is most extensive. At the hotel adjacent an excellent "5 o'clock tea" may be had. The return thence in summer time may be made by the Electric Tramway to the Box Hill Railway Station, to which it leads, whence the return to the city is a matter of 30 minutes only.

But the Rambler while on the Horse Car may desire the driver to put him down at the nearest point to an institution at Kew, well worthy of a visit—this is the Kew Lunatic Asylum. On our visit there we found no letter of introduction or order necessary to the inspection asked. For an hour or so an obliging attendant took us over the Asylum and its grounds and acted as a good guide in all respects. Whether the Melbourne

Benevolent Asylum or this Kew one may be our destination is yet in the future, but these charitable institutions, supported out of ratepayers taxes, are well worth seeing, if it be only to learn how well public money is expended and how much good is done with it. If it be desirable to make the visit to Kew Asylum in a more formal manner, a call at the Chief Secretary's office at top of Collins Street, and on the proper official there, will obtain the order, or, better still, it may be obtained by a polite note for that purpose, addressed to the Resident Superintendent at the Asylum. We lately occupied, with much interest, three months in Saturday afternoon visits to the public institutions around Melbourne, supported out of public monies, and can recommend to others a like occupation of spare time.

The Terminus of the Horse Tram Line is at the gate of the prettiest of all Melbourne's suburban cemeteries, the Boroondara Cemetery. As a great poet observed of a similar locality, it "might make one in love with death to think that one could be buried in so sweet a place." Many of Melbourne's most notable people seem to have been of this opinion. Of that we may well judge by the popular names on the graceful monuments with which this cemetery abounds. For tombstone inscriptions, in better taste than we often notice, this cemetery is quite noteworthy. The views from the well-kept grounds are very extensive and beautiful.



The Fitzroy & North Fitzroy Line.

(ROUTE No. 3.)

LEAVING this line at its terminus near to the Merri Creek Bridge, to the right is a short street, suburban for half the distance and rural for the rest, leading as it does down a grassy slope to the willow-shaded bank of the Merri Creek. In Miller Street, to left of the Tram Terminus, or rather in a siding from it called King Street, will be found one of our latest sanitary improvements—"The Destructor." A tall chimney will sufficiently point out the locality. The interior is at public service for inspection. Like the abattoirs near the Flemington Course, this serviceable if unpleasant one of our sanitary institutions will repay those curious about general public matters. "Purified by Fire" was here seen in practical form. Those undecided as to cremation may find material for thought on that subject and on others, but "The Destructor" is not at present in use.

Returning to the Tram Terminus, we have the Merri Creek Bridge before us, and a surprise also in finding two bridges side by side; these stand at a great height above the creek. The first, or oldest of the two, dates from the early years of the Fifties, and the second from those of the Eighties. The old one is a curio of bridge building, and is for foot passengers only. It is something venturesome to cross it now. Over the bridge to the left runs St. George's Road. The road in front leads to the Northcote Road, and to a staircase way of ascent to it; to the left in St. George's Road is the building formerly used for an Inebriate Asylum Institute, but now let to a private tenant.

But a little further down this road on the opposite side stands, on a hilltop, the attractive buildings known and well known as St. Joseph's House or Convent of the Little Sisters of the Poor. Admission is given at once to those desirous to see the interior. One of the nine-

teen nuns, who unassisted do all the work of this Benevolent Asylum, politely takes us in charge explaining everything, and answering inquiries in English, with a French accent. This peculiarity is accounted for by all the nuns being from France, though not all of French parentage.

The grounds of this Benevolent Asylum consist of a Government grant of 18 acres, and a glance around will show how well they are laid out. The main building is situated on an eminence affording fine views in all directions; a chapel is attached. Persons of all denominations are alike admitted to the Asylum, though the institution is founded by a Catholic body. Here, the toils of life over and most of its troubles, are between two and three hundred of old people. To be correct there were here resident at date of February, 1894, 118 men and 102 women. The qualifications for admission are—that each applicant shall be over 60 in years, and in destitute circumstances. To the most casual observer it is quite evident that those one sees here have none of them been improperly admitted. Age and penury are writ large enough on their toil-bent forms and furrowed faces. That they are cleanly kept, well fed, and well cared for, is the sole solace derived from the much here seen of what Hamlet calls "the calamity of so long life." Death it is quite evident here must strike blindfold at its victims. It is impossible otherwise to account for its taking away the young, the strong and the prosperous, as it does, and leaving here, to linger and suffer, the many bedridden and half-bedridden old people, for whom life can have no delights. The hour spent in this asylum was not, however, the least interesting of those given to visits to suburban charities, and the like interest will be probably experienced by those who may visit it.

Leaving St. George's Road on its left we may strike across the country and ramble away further to the left in the direction of Nicholson Street. The main characteristic of this quarter, as also the Northcote Road neighborhood to the right of the St. George's Road, is seen to be tall chimneys. There is a cluster of 12 of such to be seen from the back balcony of the Asylum we have left. One of its inmates called them "the 12 apostles."

Reaching Northcote Hill by the help of the stairway already mentioned, it is but a pleasant walk, and that downwards most of the way, to where the Clifton Hill Tramway awaits us (Route 4). By this we may return to town, though starting at two o'clock in the afternoon, we find it is quite six before we again reach the city. Enough of walking has been had probably to satisfy us in the many corridors and staircases of the Asylum, which occupied so much of the time.

As a midway ramble from this route, we have, on another occasion, left the Cable Tram at the Edinburgh Gardens, mentioned in "Places of Interest on Route 3." These fine gardens are of between 30 and 40 acres, are well laid out and planted, and kept in order at Government and Fitzroy Council joint expense. These gardens, especially on the walks in the direction of Clifton Hill, are well worth a ramble, the trees and shrubs being well grown throughout. Taking to these gardens from the Cable Line we emerge on the Heidelberg Road, meeting there the Cable Trams from Clifton Hill on their way to Melbourne.



The Clifton Hill & Collingwood Line.

(ROUTE No. 4.)

TURNING from Smith Street into the Heidelberg Road or Queen's Parade as it is also called, the Cable Tram Line may be left for one ramble at the junction of the Heidelberg and Northcote roads. The purpose of so doing will be to have a look at the reserves to the right of the Queen's Parade. These are known as Darling Gardens as to one and the Mayor's Park as to the other. Both are worth knowing, if only in the way of land marks in this locality.

If a seat be kept until the terminus is reached, then there are two semi-public institutions near at hand, at which more than a short look should be given. To the left of the Tramway Terminus is Rushall Crescent. If this be followed round for a few yards an agreeable surprise will be found by the ramblers coming upon the grounds of the Old Colonists' Home. This is a pleasing sight if looked at only from the roadway in front. Its cluster of some thirty pretty built cottages, with gravelled and neatly planted roadways to them, claims attention at once. Entering upon the grounds, for which a polite request to the lodge keeper only is necessary, we come to the lodge keeper's cottage, the first on the right. To this is attached a lodge room or hall for reading purposes, with the nucleus of a library for the use of the inmates of the cottages. The residential cottages are all donations. Whether of one only or three or four together, they alike bear the donors names and are pleasantly varied in styles of architecture. Each cottage is of four rooms with out-building accommodation. No difficulty is raised by the inmates on a polite request to a look around, which will be highly satisfactory to those making it. In addition to this gratis accommodation with water laid on and freedom from rates and taxes, each inmate has a weekly ten shillings allowed from the funds of the charity. Here within a hundred yards or so of the high road and the Tramway Terminus, and yet sufficiently secluded by the curve of the Crescent, these worthy old colonists have a most quiet retreat—an "Anchorage" so to call it, after the storms and changes of life which, all who see it, will agree is a very desirable resting place. Not a few will hope that they may find the like when needing it. In this respect the Old Colonists' Home much reminds one of the Charter House Retreat in the city of London, in which one room only with water and firing is allowed to each inmate, with fifteen shillings for weekly allowance.

Adjoining the Old Colonists' Home and its well kept grounds, will be found the Dramatic and Musical Retreat. Here the accommodation is in one large building and not in distinct dwellings, but such accommodation is perhaps for many reasons more companionable in its character. Here live retired those who have "strutted their hour upon the stage,"

and our hope goes with yours in trusting that they will find their rest a pleasant one in return for the amusement they have afforded to a generation passed away.

Returning to the Tram Terminus the Rambler has a choice of roads before him. In front lies Northcote, with Preston beyond, and a few yards behind him lies the Heidelberg Road from which he has lately turned.

Northcote and Preston have a continuation Cable Tramway to them, the property of another company, and a trip over this Line will open several lovely walks through Preston and surroundings.

The Heidelberg Road was that first made from the city. The "forefathers of the hamlet," that Melbourne was in the days of the Forties, mistook the way in which settlement would trend. It has gone westwards and southwards from the city and not in this direction. The Rambler will find it, notwithstanding, a pleasant road on which to ramble. Half a mile from its junction with the Northcote Road and on the other side of its Merri Creek Bridge, a turning to the right conducts one to the grounds of the Yarra Bend Lunatic Asylum. This Asylum differs from that in Kew, in adopting the separated cottage system, like that of the Old Colonists' Home we have lately left. The Kew Asylum (Route 2) is in the contrary way of one large building. Each system has its advocates and that of Kew, being the last one adopted, may be presumed to be the one having most in its favor. No objection is raised to visiting this Asylum and seeing the condition of its grounds, its cottages and their inmates, and the visit will be found to be replete with interest.

If such visit be postponed and the ramble along the road be continued to Heidelberg, a very pretty old English-like village will be found at the end of the trip. Heidelberg has not advanced very much since it was settled in the far back Forties. The cause of the settlement being here placed will be seen in the prettiness of the location and its surroundings. The Yarra appears here to much advantage, especially from the broad and unexpectedly long bridge which crosses it. Pleasant meadows lie on its other side and the road through them, if pursued, will bring the Rambler round to the Kew district.



The Nicholson Street Line.

(ROUTE No. 5.)

LEAVING the Cable Tramcar at Park Street, which is on the far side, fenced by the track of the Outer Circle railway, we cross that track and continue on the line of Nicholson Street.

But not for long! Nicholson Street comes to an end among quarries, brickworks and fenced-in paddocks. But roads remain to either side of it which may form agreeable walks to fill up an afternoon. The bluestone quarries are a great feature of this neighborhood, and it more than ever brings to the onlooker's mind the often-asked question as to whence this volcanic deposit came? Some few years back a spirited correspondence was carried on in the pages of the *Argus* on the subject. This bluestone was once lava, and came boiling out from some huge crater, whence it ran down in streams all around the north of Melbourne. None of it crossed the Yarra. South of that river bluestone is never found. As no vestige of a volcano now remains near Melbourne, the correspondence alluded to led to no decision on the subject. The question is an open one, and will probably long remain "whence came the bluestone?" or "where was its volcano?"

Right and left from Nicholson Street, near its termination here, runs Blyth Street; turning down it to the right the rambler comes shortly upon a broad and well-built bridge over the Merri Creek, which creek has here a better appearance than at many places in its course. Crossing this bridge a pleasant-looking and well-made road will take the rambler to Northcote and on to Preston, whence he may return to Melbourne by railway.

If this route be not adopted, Nicholson Street may be followed to its curved turning into Albion Street, by which route, after a mile or so of walking, the Brunswick Cable Tram Line (Route 8) will be reached. The distinctive feature of this quarter appears to be Chinamen's gardens. The industrious Mongolian may be seen on both sides of the roadway, his avocation being varied by those of the brickmaker, but the brickmaker's business it is soon seen is no such certainty as is the Chinaman's, whose green-stuff produce can always find a market.

On this Nicholson Street route we see on glancing backwards that we have omitted to mention the Convent of the Sisters of Mercy at the Junction of Palmer Street and facing the grounds of the Exhibition as one of the places to which a stranger might be attracted, and is admitted on proper application being made.

The Carlton Line.

(ROUTE No. 6.)

LEAVING the Cable Tram Line at its terminus near to Studley Park Bridge, a turning to the right there will bring the Rambler to the well-known buildings and grounds of the Convent of the Good Shepherd, a reformatory for females, and one of well-established usefulness. An application at the door to the Lady Superior will obtain admittance to the extensive grounds and information as to the laundry and other industries carried on.

Crossing Studley Park Bridge, the road is that main road which leads on to Kew, and its Horse Tram Line (Route 17). At the top of the hill, a turn to the right leads to that curious old Church Street Bridge, mentioned in Rambles from Route 2.

Taking neither of these roads we may do better for awhile by mounting the broad path leading up the bank to the left. This path leads to pretty scenery of both wood and water. A big bend of the Yarra occurs here, nearly encircling a high hill. Coming to the farther side, which is but a very short walk, we find what, in Hamlet's language, "gives us pause."

From the high ground the Yarra is seen to make a fine sweep around, and at our feet to the right are "Dight's Falls." This is in reality a cascade, but so named from a miller who took advantage, in by-gone days, of the water power here available. It is usual we see from those sitting around to bide awhile and look about. It is a grand panorama that we see when turning from the river view; Melbourne, Fitzroy and Collingwood lie spread out like a map. A winding path takes us down the hill or cliff side to the water, whence a little scrambling over stones seats us alongside the cascade, which when the Yarra is not at low water is well worth the trouble so taken.

Venturesome people and those bent on finding quiet spots for angling purposes will continue the scramble along the hill side for quite a mile all round this bend of the river. We do not recommend it, and having once tried the troublesome business ourselves do not desire to do so again. Retracing our steps to the Studley Park road from this visit to "Dight's Falls," we walk but a short distant onwards to where the road offers a pleasant descent to the meadow-like land on the river side. We may take a boating excursion, by means offered at boat houses here, or ramble for awhile along the riverside. A narrow bridge which is to be seen not far from the boathouse will take us by a backway into the grounds of the Yarra Bend Asylum. To occupy one afternoon to some purpose we took this back entrance as the way for our visit, and found no objection made to our so doing by the superintendent.

On looking over "The Places of Interest" on this Line we would suggest that on one of the trips taken upon it the car be quitted at the junction of Lygon and Grattan Streets. A short distance down this latter street and to the right of it will be found the gates of the University grounds, which are open to all as is a park. Behind the colleges will be found the greater attraction of a well-filled museum, also for public use. Especially good are the zoological and ornithological collection here to be seen; in fact, to the hall and surrounding gallery of this museum more than a cursory glance should be given. Outside of this museum is the skeleton of a monstrous whale—one of the largest ever found; at the rear of the museum is a recreation ground.



North Carlton Line.

(ROUTE No. 7.)

THE ramble from this route acquaints us well with the extension of Melbourne and how rapidly it has stretched itself in this direction. But awhile ago all that is seen before and around was waste land in much the same state in which it had been from the Creation. The foot of the aboriginal and that of the kangaroo alone knew it up to the days of the Forties. In the early years of the Fifties it was trodden by the multitude, who from most civilized parts of the world came to Melbourne, going thence in crowds over this track on gold-finding ventures. In the year 1852 we were present personally at an aboriginal encampment, whereabouts now runs before us the Outer Circle railway. It was here where houses are thick all about, carts are running and children playing, that we saw native dances and that peculiar exhibition of talent wholly confined to the Australian aboriginal—the throwing of the boomerang.

If the growth of London surprises everybody, the like may truly be said of this antipodal London, for such Melbourne really is, and for that matter more London-like in many features than any other city of the world. It was in this view of its growth that George Augustus Sala dubbed it "Marvellous Melbourne." It must be as gratifying as astonishing for those Melburnians who have adopted Victoria for their home to see such progress and what clusters of streets hereabout represent the growth of a few years. These are yearly multiplying and spreading octopus-like, clutching hold of all vacant building ground in what is known as "The Metropolitan area," in which this quarter is included.

We find ourselves, after leaving the cars and taking such look around us, enquiring as to what is ahead. We then learn that every street running northwards or in front of us would, if carried on to long length across brickfields and quarries, bring us to a cross road or great artery of traffic called Bell Street. We make our way towards this leading thoroughfare in the only practicable way hereabout of getting there by streets trending to the left into the Coburg Road. This road is reached near to its continuation of the Sydney Road. The Coburg Horse Tramway told of in Route 8 brings us, in about a mile ride, to Bell Street at its junction with the Coburg Road, and to the near side of the stockade.

Bell Street runs both left and right, and looks to either way a road on which to take a goodly walk. Taking to the left first, we wander for a good stretch down to that end of it which is at the Moonee Ponds Creek. Retracing our steps we feel half inclined to return by the tram again, at the Coburg Road Junction, as having had rambling enough for the afternoon. Curiosity overcomes us however and we start for a walk to the right or eastward direction of Bell Street. The Coburg cemetery to the left side of the street is the first object seen, passing which we mount rising ground for some little distance. Arrived at the top of this we stay to draw breath and look around at the extensive view lying, panorama-like, before us. Meeting a woodcarter, who with his load is making a halt also, we strike up a conversation.

"Where does this road, this Bell Street, lead to if we keep on the way you have come?"

"You can finish it where you like! I don't think that there is any end to it! I never found any along this way!"

"How far along have you been then?" we ask in our utter amazement.

"Only to Preston, some four miles or so yonder, but it goes on just the same after you leave that township!"

"Where can it finish then—it must have an end!" we say.

"I have been told that it goes on to Heidelberg, but not whether it ends there."

After this information it was evident that the eastward part of this leviathan street was not to be tackled after our afternoon's ramble—it evidently required a whole afternoon to itself. It was from the information received, in the way of roads one of those very lengthy ones, of which the Irishman conjectured that the other end had been stolen. Hoddle Street of some eight miles long, and this Bell Street of the like length, are, with the similarly long Bourke Road, remarkable streets, and in that way challenge pedestrian power and muchly so. We can imagine a householder on these roads when leaving home saying in familiar language that he was only "going for a walk up the street." If he went to the far, very far end, he would need no further exercise by time of his return—nor would his horse either if he rode or drove.

It is as well to remember that had we followed Bell Street down to its far or Heidelberg end, there is a railway at Heidelberg to bring us back to Melbourne; also that if we had enough of it at Preston, the like way of getting to the city awaited us there.



The Brunswick Line.

(ROUTE No. 8.)

LEAVING the car at the Royal Park Junction, a variety is offered to the passenger on either side of the line.

Those not desirous to take the Horse Car Line to the Zoological Gardens have the choice of two fine parks, the Princes on the right side, and on the left the Royal Park, for a ramble. The latter on Saturday afternoons is very lively with the many cricket matches then distributed about. Midway on the Horse Tram road to the Zoological Gardens and on the left side is an interesting stone of large size, marking a great event in the early history of Australia. Those who have gazed on the fine monumental group of Burke and Wills, near to the Princess Theatre, so conspicuously seen on Routes 4 and 5, may here see the starting place of their praiseworthy but ill-fated expedition. Up to the end of the Fifties the continent of Australia had not been crossed. It was as much entitled to be called a "Dark Continent" as was Africa at that time. An expedition had been started by a private contribution of £1000, which was supplemented by public funds, and assisted by the Government of the day. It was at the point marked by this stone that the expedition mounted on camels and horses started on their great and perilous journey. Many fell by the way—killed by its hardships. Three of the number, Burke, Wills and King, reached the borders of the Gulf of Carpentaria and thence returned. But it was only to reach Cooper's Creek as far as Burke and Wills were concerned. Want and fatigue there killed them, and King alone remained to tell the sad story. His constitution had so suffered that he survived but a few years. An Australian poet has well expressed

in verse the result of a similar but shorter expedition into unknown wilds of Australia—

"The ford at the creek was fetlock deep,
As we watched them crossing there,
The rains have replenished it oft since then,
And oft have the stones laid bare;
But the waters of hope have flown and fled,
And never from blue hill's breast,
Came they back—by sun and by sand devoured,
Where the pelican builds her nest."

To the other side of the Brunswick Line is the Carlton Cricket and Football Ground, well patronised on Saturday afternoons both summer and winter. Just beyond is the entrance to the little-frequented but pretty and well-planted Princes Park, with another well-kept Cricket Ground.

A little further from the Sydney Road in the same direction the Rambler will come to one of the sights of Melbourne, which no visitor should leave unseen. Nor should the Melbourne Cemetery fail to be periodically visited. Its monumental tombs, second to none to be seen in any cemetery, have continual additions made to them outvying in costliness and taste the many hundreds previously erected. Those who have seen the monumental stone marking the Burke and Wills expedition departure told of in previous paragraph may here see the far larger stone, of a gigantic size, which memorialises their sad death in the service of the colony. An afternoon may well be spent in this splendid "city of the dead." The old colonist, or the returned one, will see many, too many, monuments here that will tell him of more than the flight of time and recall recollections more or less pleasureable. The stranger will see simply a cemetery of which any city might be proud.

Arrived by tram at the end of the Brunswick Line, the Rambler may take the Coburg Company's Horse Cars there awaiting and go a further tram ride for a mile or so. This may be done with advantage if an order has been previously obtained from the Penal Department for an inspection of the Coburg (formerly "Pentridge") Gaol, an interesting institution well worthy of a visit by those not compelled to stay there as are some 600 of its "boarders."

It is all "Coburg" from the commencement of the Horse Tram Line to beyond the stockade or gaol, up to which the road seems to be but a continuation of the

one through Brunswick, and as thickly built with shops to either side. After the stockade is passed more of a rural character is visible to where the Horse Tram Terminus is reached at Gaffney Street. A walk down this street to the left of the Tram Terminus will bring the Rambler to Pascoe Vale Railway Station, from which a return to town may be made. The road by which Coburg is thus so easily reached was and is known as the "Sydney Road." Old colonists will well remember it as that by which, with swags on their backs and pannikins and frying pans in their hands, crowds of gold diggers wended their way in the early years of the Fifties when bound to Bendigo, Forest, and Fryer's Creek, and adjacent diggings.

At the terminus of the Brunswick Cable Tram Line the Rambler not wishing to go on to Coburg may instead take a short walk for a few yards to Moreland Grove. Turning into that tree-planted and well-built avenue, he will pass down the central street of the Moreland Estate. Passing over the cross-road at the foot of it and turning a little to the left he will see to the right a rural lane hedged to the left and to both sides further down, and redolent of sweet briar. This leads pleasantly to the banks of the Merri Creek and to a good pic-nicking spot there—the trees thereabout affording a very grateful shade. Fallen trees here and there make rustic bridges across the creek. Arrived at the other side and passing across some paddocks a ramble may be made to the right away to St. George's Road (Route 3). A walk down this road to the right will lead to the Merri Creek and to the terminus of the North Fitzroy Cable Tram Line on the other side of it.

If no such long ramble be desired a walk of a mile or so down any of the turnings to the right of the Brunswick Line will lead to Nicholson Street and its tramway, by which route, also, a return to town may be made.





The North Melbourne Line.

(ROUTE No. 9.)

LEAVING the North Melbourne Line at its terminus, a choice lies with those who would go thence on a ramble. The omnibus route through Flemington to Moonee Ponds awaits such as are not inclined for a walk.

For those who are so inclined a road lies to the right of the terminus, and at the north end of Royal Park, leading up to the Acclimatisation Society's Grounds and Zoological Gardens, and to the Model Farm and Industrial Schools. An inspection of the two latter awaits the public without payment, but, to the former, free admission is to be had only on Sunday afternoon.

To the left of the road at the Tramway Terminus runs, at right angles to it, the Racecourse Road, up which a long ramble may be made in many directions. To the left of this road are the Cattle Yards, facing the Newmarket Railway Station. A cross road again to the left, and to the west of the Cattle Yards, which the Londoner will recognise as appropriately named "Smithfield Road," leads to the Abattoirs so well known, at a distance, to all visitors to Flemington Racecourse. The Abattoirs we found to be open to the public in business hours. What is to be seen there may be as introductory to the famous Abattoirs at Chicago, seen by all visitors as one of the sights of that great city.

This Racecourse Road, a little further ahead, forms a junction with the Epsom Road and Ascot Vale Road. Following the latter road for awhile, we have Flemington to the right and the curiously-named Temperance Township to the left. We might have

gone from the junction of this road with the Epsom Road for a stroll around the Melbourne Racecourse, but "we have been there before," and, besides, it does not look inviting, but rather desolate in its unpeopled condition.

Beyond the Racecourse boundary, and on the Epsom Road, the Rambler will, on left-hand side, reach the Agricultural Society's Grounds. Facing these grounds, and on the other side of the Epsom Road, is Sandown Road, a cross road by which we again reach the Ascot Vale Road. Had we continued on the Epsom Road it would bring us to the Maribyrnong Road, turning down which fine road to the left we reach the Saltwater River (and the anglers' favourite resorts there); and turning up which to the right we come to the Ascot Vale Railway Station.

If we continue our walk on the Ascot Vale Road we pass the Ascot Vale Station, and crossing the Maribyrnong Road are in Moonee Ponds township. We may pursue the walk on the road we have been on to the Railway Station in Holmes Street, near to the junction of the high road from Flemington and Ascot Vale to Moonee Ponds, where the Tramway Company's Omnibuses come to their terminus. Returning to Town by one of these ways we make a pleasant variation of route.

At this Bus Terminus the Moonee Valley Racecourse lies to the right of the road, and is thus easily reached by the Company's Omnibuses from the Flemington Road Tramway Terminus.

From the Moonee Ponds Junction two main roads run thence ahead of us. That to the right leads to Pascoe Vale; that to the left is the wide and well-made Mount Alexander Road. It may be followed, at the will of the Rambler, to where it, some miles further on, makes a junction with the Deep Creek Road and that leading to Broadmeadows. The Rambler will probably be satisfied with getting as far on this Mount Alexander Road as where Buckley Street turns to right and left of it. Turning down to the left, the Essendon Railway Station will shortly be reached for a speedy return to town that way.

The West Melbourne Line.

(ROUTE No. 10.)

THIS is one of the shorter routes of the Cable Lines if the passenger does not connect with a North Melbourne Car at the Queensberry Street Junction, and go thence down Abbotsford Street to the Flemington Road and its terminus. If such be done, something of what may be seen beyond will be found on preceding page as Rambles from Route 9 (North Melbourne).

As rambles from this route there are three places of much interest within its short boundary.

The first is reached by leaving the cars at the junction of Lonsdale and Queen Streets. Turning to the right up Queen Street, and crossing Latrobe and A'Beckett Streets, the grounds of the Old Cemetery of Melbourne lay to the left. The enclosure is well fenced with iron railings, set in a bluestone kerbing. The entrance gate will be found on the far side from Queen Street. The visit to this resting place, of what Gray calls "the rude forefathers of the hamlet" that Melbourne really was in the days of the Forties, will be found of much interest. The visitor will not leave it so soon as might be thought. The memorials here to be seen are those identified with the foundation of Melbourne, and this graveyard might in such sense be called that of the founders of the city.

Facing to the entrance side of this old and long-closed cemetery will be found Melbourne's earliest-made reserve, "The Flagstaff Gardens." If there be another name for this reserve we have not heard it. To all Melbourneans, since the earliest years of the Forties and Fifties, it will be always known by the old name—a name which it acquired from a "look-out" over the bay erected on the highest ground of the enclosure. The arrival of ships in the bay was signalled from

here by the hoisting of a flag on a tall flagstaff by the official in charge. The look-out man and his flagstaff have long since gone; even the hill on which he stood has been cut through to allow the continuation of King Street. But the Flagstaff Gardens remain for recreation purposes, and, with such association connected with them, are worth a visit from those who would know points of note about the Metropolis of Victoria.

Leaving the Flagstaff Gardens, a short walk will take the Rambler to another point of interest; or on a tram ride by this route the car may be left at junction of Spencer and Miller Streets. A few yards up the latter street, and a turn to the left, will bring the Rambler to the gate of the Melbourne Benevolent Asylum, one of Melbourne's oldest and most valued institutions, dating back before the days of the gold diggings.

The Melbourne Benevolent Asylum is always open to the inspection of visitors. Its genial superintendent, Mr. Stobie, deposes an attendant to show the visitor around. This excellent Institution receives and shelters, feeds and clothes, wholly out of charity, between six and seven hundred inmates of both sexes. The women portion are in the minority in this number. "Missing friends" may sometimes be discovered in a ramble through this Asylum—poverty often so hides itself. Of those who have "seen better days" the number here to be found are many indeed, who, "wearied with disaster, tugged by fortune," come here, as weather-beaten crafts, to a harbour of refuge.





The Prahran Line.

(ROUTE No. 11.)

FROM the terminus of this route the rambler may take a look over the grounds and buildings of the new Town Hall of St. Kilda, which lies to his immediate right on leaving the car.

Taking the first turning to the immediate left of the terminus, we are in the Balaclava Road, with the railway bridge crossing it and the station to the right-hand side.

The Balaclava road and its adjacent ones afford fine rambles and over well-made footways. It lies equidistant from the Dandenong Road to the left and Glen Huntly Road to the right. Either of these roads are to be reached by right or left turnings for that purpose, taking to Hotham Street, which is the first cross-road reached, or down the next one, the Orrong Road, which may be followed to the left, away past the Toorak Road to the south bank of the Yarra. The Boundary Road, which is the next one met with, may be followed in the same direction with a like result.

The next two cross-roads met with are the Hawthorn Road and the Bambra Road. Between these two and to the left of the Balaclava Road lies the Public Reserve, known as "Caulfield Park." If we follow up this Balaclava Road to its long end we find it finishing at its junction with the Inkerman Road, which here diverges to the right. Parallel with the Balaclava Road, and to be reached by either of the cross-roads just named, run on the left Alma Road (next to Dandenong Road) and Inkerman Road, and on the right the Glen Eira Road.

At the head of the Balaclava Road where it merges into the Inkerman Road, a short walk along the last-named thoroughfare brings the Rambler to the Caulfield Racecourse Station, from which a divergence may be made to the racecourse itself or to the adjacent popular suburb of Caulfield. A return to town may subsequently be made from this railway station.

This Prahran Line connects at Chapel Street Junction with the Bus Line up Dandenong Road (Route 21). On this route lies to the right the St. Kilda Cemetery, a much admired place of burial, and one holding some very fine mortuary memorials. If this cemetery be entered from the Dandenong Road, an exit may be made at its farther end on the Alma Road. The termination of this Bus Line is at the corner of the Sutherland Road, near to the junction of the Wattletree and Dandenong Roads.

The Toorak Line.

(ROUTE No. 12.)

THE rambles from this road are very pretty ones, and near to town. The cars may be left on the Domain Road, before the turn into Park Street, for a stroll through the Botanical Gardens, and thence down the south side of the Yarra, passing Brander's Ferry to Princes Bridge. Or, passing to the farther side of the Gardens, and crossing the Botanical Gardens bridge, the north bank of the river may be reached. The road along this route to Princes Bridge passes the well-designed Morgue Building, an inspection of which is open to the public, and deserves a call. It compares very favorably indeed with the Parisian Morgue, as the visitor who has seen the latter will verify.

Passing up Park Street, which joins the Domain Road, to the Toorak Road, the cars may be left at the gate of Fawkner Park facing Park Street. A stroll towards the centre of this park will bring the visitor to the "Diamond," or junction of seven roads there. Each of these may interestingly be followed to its

outlet; one of them, the second to the right of that following down from Park Street, leads out fronting the Commercial Road and the Alfred Hospital—one of Melbourne's noble charities, always open to public inspection.

If the Toorak Line be left at its terminus (the Irving Road), the strolls down the roads right and left present many views of interest, to say nothing of the beauty of the villas seen thereabout. The third road running right and left from the Toorak Road, beyond Irving Road, is a noble thoroughfare of some miles in length. This is the Glenferrie Road. To the right it will take the Rambler away to Malvern and its Bus Route (No. 20), and by that road to Chapel Street Tramway on Route No. 11. Some of the notable mansions on the left of this Glenferrie Road on the way to Malvern "gave us pause," as Hamlet puts it. We had, perforce, to stay and admire for awhile, and then enquire as to the owners of such wealth of which these mansions so plainly indicated the possession. This fine road may be followed on past Malvern to its junction with the Wattletree Road, and a return thence be made to the Dandenong Road and by its Bus Line (No. 21), which connects with the Cable Tramway at Chapel Street (Routes 12 and 14).

If the Rambler turns to the left or western side from the Toorak road, the Glenferrie Road can then be followed right away into Kew and to its junction beyond with the Cotham Road. The return thence to town may be made by the Horse Tram (Route No. 17) to the Victoria Bridge, and thence to town by the Cable Tram Line (Route 2).

If a further and longer ramble be sought, or one for another afternoon's outing, the second road running both right and left from the Glenferrie Road will take to it. Turning into it to the right and following it down to its intersection with the Malvern Road, a turning to the left into that road will soon bring the Rambler to another notable cross road, longer, if anything, than the Glenferrie one. This great connecting artery of traffic is called the Burke Road. Following it to the right its Eastern end that way will be found in the Dandenong Road near to Caulfield; near, also, to the junction of the Oakleigh and Dandenong railway line with the Glen Iris line. For a return on this

route we followed the Dandenong Road down to Caulfield Racecourse, returning by the railway to Melbourne.

This Burke Road is a truly long thoroughfare, as we found in following it to its other or western end. This will be found, at the end of a long walk to it, fit only for a cool day, at the outskirts of Kew where it junctions with the Bulleen Road and Kilby Road. As a mode of returning to Town, a railway station will be found down the Bulleen Road in the Kew direction; one, also, at Camberwell, and another at Malvern, both close to this road. There are nearly fifty different streets and roads running into Burke Road in its long course.

On the Toorak Road the tram conductor will point out "Toorak House," the old time residence of the Governors of Victoria, now otherwise occupied.

The Rambler on this route may as well remember that all turnings to left of the Toorak Road up from Chapel Street to its Terminus on the hill, lead away down to the bank of the Yarra, which is here hilly and steep. Such turnings as Kensington Road, Williams Road, Grange Road, Orrong Road, Lansell Road, and St. George's Road afford, at the far ends, a picturesque river view, but, as no reserve of water frontage was made on the sale of the land here, the river bank cannot, on this side of it, be traversed from one road to another. A singular occurrence happened lately on one of these roads. An over-sleepy cabman allowed his horse and cab to stray down it and topple over the very steep river bank. The man, fortunately, fell out, and, rolling down to a protruding ledge, lay there till daylight. Of the horse and vehicle nothing could be seen, and both were supposed to be at the bottom of the Yarra. The cab was fished up thence with the harness hanging to it. The horse had somehow shaken himself free, and was found grazing uninjured in Richmond Park on the other side of the river. Before noon on that day cabman, cab, and horse, were returning to town none the worse for this almost miraculous escape.



The St. Kilda Line.

(ROUTE No. 13.)

ALL along the line of this route from where the Domain Road turns from it to the left, are to be found matters of much interest to the metropolitan public, equally with the up-country and inter-colonial visitor. The left-hand side of the St. Kilda Road, going from the city, is lined with public and charitable institutions.

Nearly facing to the Domain Road turning we enter upon the Albert Park, in which to the left and half way to St. Kilda, is situated the Warehousemen's Cricket Ground, and at the far end on that side is the St. Kilda Cricket and Football Ground, which contains a specially-laid asphalt track for bicycle sports. To the right of the road through the park lies the splendid Albert Park Lake, having a marginal measurement of nearly five miles. At each end of this lake, which in no place has more than four feet of depth, boat-houses supply both sailing and rowing craft, with attendants if required. Exercise may be thus taken in either line with a safety seldom to be found in the aquatic way. More than one boating club has, at the north end of the lake, its special club house.

If we continue on the high road in preference to entering the park, we find, on passing the Church of England Grammar School and the Tramway Engine House, the Fawkner Park is at our service on the first or second turning to the left. Of this park we have told in notes on Route 12.

We next pass on same side of the road a series of Public Institutions, each of which is well worthy of a visit, and to each of which the visitor may obtain admission on a polite request to the superintendent in charge. These are the Alfred Hospital, the Wesley College, the Deaf and Dumb Asylum, the Blind Asylum, and lastly, the Jews Alms Houses.

If the Tramway Line be followed to its termination on the Brighton Road, a short walk thence to the left will bring us to the Balaclava Road junction with Chapel Street and the Prahran Cable Tram Route. The journey through Prahran and down the Toorak and Domain Road on Route 11 may be made from here, and a pleasant variation of the return trip to the city be thus had.

Or, if desired, this digression from the one route to the other may be made at the junction of Wellington Street and Fitzroy Street, St. Kilda. By taking the line up Wellington Street to its Chapel Street junction with Prahran Route, a transfer ticket may be there obtained, which will be good for the whole distance to town on that Route (No. 11).

Our notes on the Fitzroy Street and St. Kilda Esplanade Route will be found on next page (Route 14).

If a ramble be desired from the terminus of this route onwards, a stroll up the Brighton Road will take the rambler to Elsternwick, lying to the left and up the Glen Huntly Road. Here the Elsternwick Railway Station, the line to which has to be crossed to the township, furnishes a speedy way of return to town. Down this Glen Huntly Road and through Elsternwick runs a private Horse Tram Line, the cars on which will take the rambler for a two-mile run to the Glen Huntly Station on the Mordialloc line of railway. From here, in preference to the Elsternwick line, a return to the city can be made. The trains on this latter route are not however run at such short intervals as are those on the other line. Patience is often much tried in that way.





St. Kilda Esplanade Line.

(ROUTE No. 14.)

TAKING to this line from the Wellington Street Junction on Prahran Line (No. 11) at Windsor, or at Fitzroy Street Junction on the St. Kilda Route (No. 13), we pass down Fitzroy Street to the St. Kilda Railway Station, and thence onward, passing the north end of Acland Street, to the Esplanade—facing the Bay.

Two roads run round the Esplanade—an upper and lower one; it is on the upper one that the Tramway Line is laid. If the cars be here left, as they mostly are, the ramble along the upper route brings us round to the south end of Acland Street and the terminus of this route there, near to the Village Belle Hotel.

The Esplanade is well furnished with seats facing both inland and to the bay. The roadway is broad and well lighted and asphalted. On a reserve in the centre is a flagstaff, and on gala occasions a band plays there. The Esplanade and its seats will be found to be well filled with visitors on Sunday afternoons and evenings, as, also, on every evening when the warmth of the weather suggests taking a breath of sea air. It is on such evenings that the Esplanade cars are eagerly sought, and especially so, the dummy seats on them.

The lower road, taken to at the turning from Fitzroy Street, leads away round by the bathing-houses to Acland Street Terminus similarly with the upper road. Shortly after entering upon it, a turning to the right brings one on to St. Kilda Pier, which, in a quarter of a mile or so of broad plank road, well fenced and lighted, takes us to a right-angled continuation of it, also of good length. Here, as also all the way up the pier and on either side, are comfortable seats provided for the public. A shelter shed, or

pier house, will be found on the jetty, with conveniences attached, and seating all around. When at the far end of this pier visitors have the sea breeze from all quarters, being, in fact, quite a quarter of a mile from the shore, the lights on which, of an evening, glimmer in the distance.

The bathing accommodation of the bath-houses, for both sexes, on the lower Esplanade road, is equal to that to be found anywhere. The Baths are lighted by gas all along the line of bathing boxes, and that up to a late hour of the evening, and hot as well as cold salt-water baths can be had at all hours.

As a variation of route on returning to the city, that by way of Beaconsfield Parade may be taken. This road runs along the sea beach in a well-asphalted double pathway, the inner one being the footpath fronting the villas which line the whole route away to Port Melbourne. A Convent, on the plan of that of the Good Shepherd at Abbotsford, will be found midway on this parade. A twenty minutes walk from St. Kilda will bring the Rambler to Victoria Avenue, at which is the Cable Tramway Terminus of South Melbourne Line (No. 15). By this an agreeable change of route may be made to the city.

Beyond the St. Kilda Terminus of this line, a ramble to the right may be made to the well-known Red Bluff and the settlement known as Elwood just beyond. From Elwood, turning to the left, the Brighton Road can be reached, and a return to town be made by the St. Kilda Cable Tramway Line (Route 13).



The South Melbourne Line.

(ROUTE No. 15.)

IF this route be left at the turning from Clarendon Street into Park Street, a walk along Clarendon Street may be taken to Albert Park, a gate of which faces Clarendon Street. Immediately within the fence will be found the South Melbourne Cricket and Football Ground, as also the boating houses, and Rowing and Yachting Club Houses on the north shore of the Albert Park Lake, which here stretches away in front to St. Kilda and its Esplanade Tramway Line (Route 14).

From this park and lake a divergence may be made to the St. Kilda Road on the left, and the tramway on that road be taken either to the Brighton Road Terminus or to town.

If the route to South Melbourne be continued to its terminus on Beaconsfield Parade, we find ourselves on the beach. To the right stands the Pavilion, in which on Tuesday and Thursday evenings throughout the summer months the "Tramway Band" will be found playing from eight till ten o'clock—an entertainment well patronised by the general public while seeking an evening's change from city air.

Further to the left will be found the South Melbourne Pier—an agreeable promenade.

Bathing houses for both sexes are situated on this beach, and are well fenced in. All requirements in the way of bathing dresses and swimming belts are to be had from the attendants.

A walk to the right of the terminus takes us along this beach frontage, and on a well-asphalted road to Port Melbourne, its Town Pier and Railway Pier. From this point a variation of route may be made by taking the Port Melbourne Cable Tramway Line (Route 16) back to Melbourne.

Or the ramble may be made to the left of the terminus, passing the South Melbourne Pier and along the Beach Road—a broad and well-asphalted one, to St. Kilda and its Esplanade Tramway Line (Route 14). A return to the city may be made from thence by way of the St. Kilda Road (Route 13), or on a transfer ticket up Wellington Street to the Chapel Street junction and so to town by way of the Prahran Route (No. 11).

The Port Melbourne Line.

(ROUTE No. 16.)

THIS line takes us over the Queen's Bridge. On the city side of it will be found the wharves, at which are the intercolonial steamers, trading thence to Sydney, Adelaide, Tasmania and New Zealand. The steamers for Geelong, Portarlington, Queenscliffe, Portsea, Sorrento, Dromana and Schnapper Point (Mornington), also leave from this side to their different destinations.

On the opposite or south side of the bridge, similar extensive wharf frontage is provided for the many craft lying there and starting thence. The Union Line of New Zealand steamers start from this side, and, among others, the "Coogee," of the Tasmanian Line.

The Port Melbourne Road is thoroughly a business one, and its vehicles are those mostly in connection with shipping traffic.

At its terminus we find the two piers—the Town Pier and the Railway Pier. Before the Melbourne Harbour Trust had so much improved the river, as it has done, these piers were always crowded down each side with the vessels which now make their way up river to the wharves. The Railway Pier is, however,

yet the place of embarkation and debarkation for the mail steamers of the P. and O. and Orient Lines; also, for those of the French and German Lines, and for those of the Port Line and several others. On Saturdays at noon, or thereabout, the Railway Pier presents a busy sight on the departure of one or more of these magnificent vessels. A visit to it on such occasions will be found very interesting to the up-country visitor to the metropolis.

At every hour from 8 to 6 a steamer leaves this pier for Williamstown on the other side of the bay—a passage of about a quarter of an hour. This sea-side suburb will be found especially well worthy a visit. Williamstown is a port and municipality, distant some nine miles from Melbourne by its overland road. For long time the mail steamers made it their port on arrival and departure, and its piers are seldom without shipping at them. The Graving Dock, at Williamstown, accommodates the largest vessels, having an entrance of 80 feet in width, and being 480 in length, and it is now being further lengthened. The first stone of this great work was laid by the Duke of Edinburgh in 1868. Vessels from other colonies come round here for the accommodation this dock affords for repairs. Batteries, well mounted with large guns, protect the outer shore of this port. The Railway Workshops are the largest in the colony, and the Williamstown Racecourse has a special line of railway to it.

The Back Beach of Williamstown, to the other side of its railway line, will be found well worth rambling to. It is wholly of stony character—no sand being seen there.

The return from Williamstown may be made by steamer to Port Melbourne, and thence by tramway to the city, or by way of the Williamstown railway to Spencer Street Terminus, where the tramway cars again await the passengers on routes to Richmond, Fitzroy, Collingwood, Nicholson Street, and Clifton Hill, connecting with other suburban lines at Swanston and Elizabeth Streets.

The Kew Horse Line.

(ROUTE No. 17.)

Connecting with Cable Tram Route No. 2.

LEAVING this route at Studley Park Road Junction, a ramble may be made along this pleasant road. To the right will be found, lying below, the meadow-like margin of the Yarra, which may be followed round to the grounds of the Kew Asylum. The Yarra Bend Asylum may be previously reached, at its back entrance, by a bridge which crosses the river thither soon after the Studley Park Road has been left; or, at the foot of the descent from the road, a boat may be taken at the neighbouring boat-house for a row to either way up or down the river.

Further along to the end of the Studley Park Road, a branch of it, to the left, will take us to the old Church Street Bridge, over which a penny toll is taken.

If this junction be passed, the road leads away down to the Johnston Street Bridge. To the right of this, passing up an inclined walk and ascending some slightly-rising ground, the Rambler will come to Dight's Falls—a cascade of the Yarra much admired. Descending to the river here the side of the Falls may be reached over some stony ground. Anglers and the more venturesome will ramble, or rather scramble, farther on around the loop which the winding of the Yarra here makes.

Returning to the road again the return to town may be made by crossing the bridge to the Carlton Cable Line which, near here, has its terminus (Route 6).

If the turning to the Studley Park Road be not made, the Horse Tramway Route may be continued to the roads told of in Rambles from Route 2, or a visit be made at its terminus to the Boroondara Cemetery more fully noted in remarks on that Route (No. 2).

The Hawthorn Horse Line.

(ROUTE No. 18.)

Connecting with Cable Tram Route No. 1.

THIS line at its commencement offers an inspection of St. James' Park to the immediate right on the farther side of the Yarra. Here stood formerly the residence, with well laid out grounds attached to it, of Sir James Palmer, the first President of the Victorian Legislative Council.

The route then runs into Hawthorn township and turns shortly to the right, into Power Street, and then to the left into the Riversdale Road, lined on either side with notable villas, those on the right having their rear to the Yarra bank.

The terminus of this route is at its junction with Auburn Road, but the Rambler may continue a walk along Riversdale Road for a long distance, crossing many roads on the way. An idea may be well formed here of the extension of the suburbs around Melbourne in this direction.

To the right or left of the terminus of this line the Auburn Road offers equal attractions to the Rambler. If the left-hand route be taken the Burwood Road will be reached, passing which a railway station shortly affords an opportunity of reaching town again.

Hawthorn lies midway between Kew to the left and Malvern on the right. The roads in these directions are numerous. To the left the Rambler will find the Kew Horse Tramway (Route 17) at his service for a return to the city; while to the right, at Malvern, the omnibus service on Route 20 will bring him back to the Cable Tramway in Chapel Street (Route 12).

Royal Park and Zoological Gardens' Line.

(ROUTE No. 19.)

Connecting with Cable Tram Route No. 8.

THIS line from the junction of the Royal Park and Sydney Road takes us, in a short run, to the gates of the grounds of the Victoria Acclimatisation Society. These grounds are free to the public on Sunday afternoons only. The choice and varied collection of animals and birds here to be seen compares favorably with any such beyond what London and Paris can show.

Adjacent to these gardens are the grounds of the Model Farm, to which a visit may very well be made, and especially by those interested in agricultural matters.

Also, to the rear of the gardens will be found the extensive building and grounds of the Industrial Schools. A visit to this may be made by those interested in Orphan Asylums and Reformatories. Both sexes are received and educated here, and not a few of those who have been so brought up have become very useful colonists—some few most creditably so.

A return from these visits may be made by crossing to the Flemington Road to the left of the entrance to the Zoological Gardens. Arrived there, the North Melbourne Cable Tramway will be found, by which a quick return may be made to the city on Route 9.

Malvern or High Street Omnibus Line.

(ROUTE No. 20.)

Connecting with Cable Tram Route No. 12:

THIS line runs from the Prahran Town Hall, Chapel Street, on Route 12, where it connects with the Prahran Cable Tram Line.

Some half way up High Street a railway line is crossed (at Sutherland Road Junction). Leaving the cars here the Armadale Railway Station will be found just handy to the right of this line.

The terminus of this Omnibus Route is at the Town Hall, Malvern. If the road already travelled be continued for a walk it will bring us on the right to the fine building of the Convent of the Sacred Heart. A walk down the road on which this building stands soon brings us to the Caulfield Railway Station. If a turn be made down the first road to the left, after passing the convent, a railway station on the Glen Iris will be reached. But so to do is not recommended, as the trains on this line run at long intervals, and an undesirably long wait might be experienced. See also Rambles from the Toorak Line (Route 11).

Dandenong Road and Armadale Omnibus Line.

(ROUTE No. 21.)

Connecting with Cable Tram Routes Nos. 12 and 13.

THIS line connects with Routes 12 and 13 at junction of Chapel Street and Wellington Street.

On its right about half way up lies the St. Kilda Cemetery. The grounds have an outlet at the farther end to the Alma Road.

At the terminus of the line, or near to it, is the junction of the Dandenong Road with the Wattle-tree Road. It is at the junction of this latter road with Sutherland Road, Armadale, that the bus finally stops.

The Dandenong Road may be followed for a ramble up to Caulfield and its racecourse, and a return from thence be made from the railway station there.

Or the Wattletree Road may be followed, after leaving the bus, for half a mile to where the Malvern Railway Station will be found a short way off to the right of this road and near its junction with the Glenferrie Road.

Flemington, Ascot Vale,

AND

Moonee Ponds Omnibus Line.

(ROUTE No. 22.)

Connecting with Cable Tram Route No. 9.

THIS pleasant omnibus ride through three suburbs begins at Flemington Bridge near the Tramway Terminus of the North Melbourne Line (Route No. 9), with which the West Melbourne Route (No. 10) also connects.

The Bus Route from the bridge and over the creek to the first road on the left travels the old route to the Flemington Racecourse, as colonists of the years in the Fifties and Sixties will well recall. Passing that turning we proceed through the Township of Flemington, the route being the old Mount Alexander Road, precious, or otherwise, in the memories of the early gold diggers. This was the coaching road also to Castlemaine and Sandhurst, or to Forest Creek and Bendigo as they were then called, in the days of "King Cobb and Co.," the leading coach proprietors and mail contractors of the Fifties.

The terminus will be found at the junction of this road with the Pascoe Vale Road told of on Rambles from Route 9. We have now passed through Ascot Vale and are in Moonee Ponds Township. To the right of this terminus lies the Moonee Valley Racecourse, and to the front the Essendon Town Hall. Either of the junction roads may now be followed for a ramble. That to the right is the Pascoe Vale Road, and two miles ahead will be found the Pascoe Vale Railway, from which a return to town may be made.

Of the Mount Alexander Road we have told in concluding paragraph of Rambles from Route 9.



Church Street Omnibus Line.

(ROUTE No. 23.)

Connecting with Cable Tram Route No. 11.

THIS route connects the Toorak Road with Swan Street, Richmond.

Any of the streets to the North from Swan Street will bring us on to the Richmond Road and its Cable Tramway to the bridge at one end or to the city on the other. A walk up Swan Street to the right or east will bring the rambler past the Burnley Railway Station to the Richmond Park, and the pleasant rambles on the Yarra Bank, told of in Rambles from Route No. 1.

